



TOLL BRIDGE PROGRAM OVERSIGHT COMMITTEE

MEETING MATERIALS

September 4, 2008

CALTRANS

BAY AREA TOLL AUTHORITY

CALIFORNIA TRANSPORTATION COMMISSION





Letter of Transmittal

TO: Toll Bridge Program Oversight Committee
(TBPOC)

DATE: August 27, 2008

FR: Program Management Team (PMT)

RE: TBPOC Meeting Materials Packet – September 4, 2008

Herewith is the TBPOC Meeting Materials Packet for the September 4 meeting. The packet includes memoranda and reports that will be presented at the meeting. A Table of Contents is provided following the Agenda to help locate specific topics.

TBPOC MEETING
September 4, 2008, 10:00 a.m. – 1:00 p.m.
New Benicia-Martinez Bridge Administration Building, Training Room
70 Mococo Road, Martinez

| Topic | Presenter | Time | Desired Outcome |
|---------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|----------------------------------------------------------------------------------------------------------------------------------------|--------------------------------------------------------------------------------|----------------------------------------------------------------------------------------------------------------|
| 1. CHAIR'S REPORT | W. Kempton, CT | 5 min | Information |
| 2. CONSENT CALENDAR a. July 10, 2008 Meeting Minutes* b. July 31, 2008 Conference Call Minutes* c. Revised 2008 TBPOC Meeting Calendar* d. 2009 TBPOC Meeting Calendar* | A. Fremier, BATA A. Fremier, BATA A. Fremier, BATA A. Fremier, BATA | 1 min 1 min 1 min 1 min | Approval Approval Approval Approval |
| 3. PROGRESS REPORTS a. Draft August 2008 Monthly Progress Report*** | A. Fremier, BATA | 1 min | Information |
| 4. PROGRAM ISSUES a. Opportunity Schedule Update* | T. Anziano, CT | 20 min | Information |
| 5. SAN FRANCISCO-OAKLAND BAY BRIDGE UPDATES a. Self-Anchored Suspension Superstructure (SAS) 1) China Update 2) TBPOC China Itinerary* b. Yerba Buena Island Detour (YBID) 1) Update 2) Contract Change Order 140* c. Yerba Buena Island Transition Structures (YBITS) No. 1* d. Oakland Touchdown (OTD) No. 1 1) Update e. Bridge Aesthetics Update* | T. Anziano, CT S. Maller, CTC T. Anziano, CT T. Anziano, CT T. Anziano, CT T. Anziano, CT C. Endress, CT | 15 min 10 min 15 min 10 min 10 min 10 min 30 min | Information Information Information Approval Information Information Information |
| 6. NEW BENICIA-MARTINEZ BRIDGE a. Tour (time permitting)* | M. Pazooki, CT P. Lee, BATA | 45 min | Information |
| 7. OTHER BUSINESS | W. Kempton, CT | | n/a |
| Next TBPOC Meeting: October 1, 2008 ZPMC, Changxing Island, China | | | |

* Attachments

** Final Documents still in process; to be provided as soon as available.

***Stand alone document included in the binder.

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TBPOC MEETING September 4, 2008

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| 3 | 3 | PROGRESS REPORTS a. Draft August 2008 Monthly Progress Report*** |
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| 6 | 6 | NEW BENICIA-MARTINEZ BRIDGE a. Tour (time permitting)* |
| 7 | 7 | OTHER BUSINESS |

* Attachments

** Final Documents still in process; to be provided as soon as available

*** Stand-alone document included in the binder

ITEM 2: CONSENT CALENDAR

Memorandum

TO: Toll Bridge Program Oversight Committee (TBPOC) **DATE:** August 27, 2008

FR: Andrew Fremier, Deputy Executive Director, BATA

RE: Agenda No. - 2a, b
Consent Calendar
Item- July 10, 2008 Meeting Minutes
July 31, 2008 Conference Call Minutes

Recommendation:

APPROVAL

Cost:

N/A

Schedule Impacts:

N/A

Discussion:

The Program Management Team has reviewed and requests TBPOC approval of the minutes for the July 10, 2008 meeting and the July 31, 2008 conference call.

Attachments:

July 10, 2008 Meeting Minutes

July 31, 2008 Conference Call Minutes

ITEM 2: CONSENT CALENDAR

- a. July 10, 2008 Meeting Minutes



TOLL BRIDGE PROGRAM OVERSIGHT COMMITTEE

CALTRANS BAY AREA TOLL AUTHORITY CALIFORNIA TRANSPORTATION COMMISSION

MEETING MINUTES

July 10, 2008, 10:00 AM – 1:00 PM

Mission Bay Office, 1906 Conference Room, Pier 7, 325 Burma Road, Oakland, CA

Attendees: TBPOC Members: Will Kempton, Steve Heminger, and John Barna
PMT Members: Tony Anziano, Andy Fremier, and Stephen Maller
Participants: Bill Casey, Bob Coupe (CCM), Clive Endress, Mike Forner, Dan Hemick (CCM), Beatriz Lacson, Richard Land, Peter Lee, Mika Miyasato, Bart Ney, Dina Noel, Bijan Sartipi, Ken Terpstra, and Jason Weinstein

Convened: 2:50 PM

| Items | Action |
|--------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|
| 1. CHAIR'S REPORT <ul style="list-style-type: none">a. The Chair urged everyone to pay close attention to the State budget process.<ul style="list-style-type: none">• The suspension of Proposition 42 could potentially happen (\$15-17B allocated for transportation and related work could be suspended). | |
| 2. CONSENT CALENDAR <p>BATA presented the following for TBPOC approval:</p> <ul style="list-style-type: none">a. June 18, 2008 TBPOC Meeting Minutesb. June 27, 2008 TBPOC Conference Call Minutesc. July 1, 2008 TBPOC Conference Call Minutesd. Revised 2008 TBPOC Meeting Calendar | <ul style="list-style-type: none">• The TBPOC APPROVED the June 18, 2008 TBPOC Meeting Minutes and the June 27 and July 1, 2008 TBPOC Conference Call Minutes, as presented.• The TBPOC APPROVED the Revised 2008 TBPOC Meeting Calendar as of July 02, 2008, as presented. |
| 3. PROGRESS REPORTS <ul style="list-style-type: none">a. BATA noted that the PMT approved the June 2008 Monthly Progress Reports through delegated TBPOC | <ul style="list-style-type: none">• The TBPOC confirmed APPROVAL of the June 2008 Monthly Progress Report |

(continued)

| Items | Action |
|---------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|------------------------------------------------|
| <p>authority on July 1, 2008.</p> <ul style="list-style-type: none">• As soon as updated expenditure and latest comments are incorporated, the final version of the July 2008 Monthly Progress Report will be approved by the PMT through delegated TBPOC authority.• The first draft of the Second Quarter Report, June 30, 2008 was distributed to the TBPOC and PMT members. | <p>through delegated authority to the PMT.</p> |
| <p>4. SAN FRANCISCO-OAKLAND BAY BRIDGE UPDATES</p> <p>a. Self-Anchored Suspension (SAS) Superstructure</p> <p>1) China Update</p> <ul style="list-style-type: none">• The TBPOC and PMT met with the ABF Board before the regular TBPOC meeting. The following comments were offered as a result of that meeting:○ It is important for all parties involved to effectively communicate with each other about everything concerning the project, with the TBPOC being apprised accordingly.○ While expressing openness to it, the TBPOC will not act on the ABF incentive and delay mitigation package to ZPMC relating to the schedule, until the schedule benefits are specified.<ul style="list-style-type: none">➤ ABF plans to proceed with presenting the ABF proposal to ZPMC. ABF also indicated that the incentive package could be presented in the context of a claim in the future.○ The TBPOC supported the green-tag process in concept.○ The Program will provide a design presence in China and | |

(continued)

| Items | Action |
|---------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|--------|
| <p>pick a person with the suitable skills and competence. This representative will confer with the appropriate parties to discuss the acceptance criteria with respect to the fabrication of steel.</p> <ul style="list-style-type: none">○ The need for a commitment on both sides to make the process work better was recognized.○ While a turnaround quicker than currently experienced is desired, it should be achieved without detriment to quality, the main goal. <p>b. Yerba Buena Island Detour</p> <ol style="list-style-type: none">1) East Tie-In Update, with CC Myers (D. Hemick and B. Coupe)<ul style="list-style-type: none">• Dan Hemick of CCM indicated that their latest, reasonable estimate for the East Tie-In Roll-out/Roll-in (ETI RORI) is the end of September. This is based on certain, not too conservative assumptions, and a lot of unknowns.<ul style="list-style-type: none">➤ They anticipate being off the Island 10 months after RORI.• The Department noted that the Design Team is working around the clock on this, that majority of the design is complete, and anticipates being on schedule at year-end.• Per CCM, achieving a Memorial Day date is unrealistic due to material, fabrication and erection issues.• CCM indicated that adjusting this schedule would be best discussed at the end of this year and any discussion before then would be premature.<ul style="list-style-type: none">➤ By year-end, there is the possibility that the ETI | |

(continued)

| Items | Action |
|------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|--------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|
| <p>RORI could occur during the Labor Day Weekend.</p> <ul style="list-style-type: none"> The TBPOC will revisit this subject with CCM in December 2008. | |
| <p>c. Yerba Buena Island Transition Structure No. 1</p> <ul style="list-style-type: none"> The Department noted that the open issues on this contract are being tracked according to the matrix developed for this purpose. RTL dates were met. Bids are due in January 2009. Keeping in mind that CCM will clear the Island in 2010, it was suggested that the Department get a sense of how this contract ties in with that event, determine the issues (per the matrix) and cost them out. The Department indicated that the Schedule Team has developed a graphic on how the contract ties will play out. | <ul style="list-style-type: none"> The Department to determine how to represent YBITS No. 1 in the context of the CCM demobilization schedule and report back to the TBPOC. |
| <p>d. Oakland Touchdown No. 1</p> <ul style="list-style-type: none"> The Department noted that safety and storm water issues are being managed but they remain a concern. | |
| <p>e. Gateway Park Area Visioning Conference</p> <ul style="list-style-type: none"> The conference having just concluded prior to this meeting, the TBPOC deemed it too fresh in everyone's mind to require a debriefing. | <ul style="list-style-type: none"> The PMT to update the TBPOC on the results of the Working Group efforts. |
| <p>f. Bridge Aesthetics</p> <ul style="list-style-type: none"> Item deferred. | |
| <p>g. West Approach</p> <p>1) The Department presented, for</p> | <ul style="list-style-type: none"> The TBPOC APPROVED CCO 13, Supplement 11, as |

(continued)

| Items | | Action |
|---------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|-----------------------------------------------------------------------------|---------------|
| TBPOC approval, CCO 13, Supplement 11, in the amount of \$2,000,000, for additional manpower requirements for traffic control for the project. <ul style="list-style-type: none">• This supplement will be financed from the contingency fund allotted to the contract.• As the project winds down, a good handle on the remaining work and control on cost was suggested. | | presented. |
| 5 | OTHER BUSINESS <ul style="list-style-type: none">• N/A | |

Adjourned: 4:46 PM

MEETING MINUTES

July 10, 2008, 1:30 PM – 4:00 PM

Mission Bay Office, 1906 Conference Room, Pier 7, 325 Burma Road, Oakland, CA

APPROVED BY:

WILL KEMPTON, Director
California Department of Transportation

Date

JOHN F. BARNA, Jr., Executive Director
California Transportation Commission

Date

STEVE HEMINGER, Executive Director
Bay Area Toll Authority

Date

ITEM 2: CONSENT CALENDAR

- b. July 31, 2008 Conference Call Minutes



TOLL BRIDGE PROGRAM OVERSIGHT COMMITTEE

CALTRANS BAY AREA TOLL AUTHORITY CALIFORNIA TRANSPORTATION COMMISSION

CONFERENCE CALL MINUTES

July 31, 2008, 2:00 PM – 2:30 PM

Attendees: TBPOC Members: Will Kempton, Steve Heminger and John Barna
PMT Members: Tony Anziano, Andy Fremier, and Stephen Maller
Participants: Michele DiFrancia, Beatriz Lacson, Peter Lee, Dina Noel, and Bijan Sartipi

Convened: 2:03 PM

| Items | Action |
|---------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|---------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|
| <p>1. Second Quarter Report, June 30, 2008</p> <ul style="list-style-type: none">• Discussion/comments included:<ul style="list-style-type: none">○ Change the SAS Superstructure from a green light to a yellow light on Table 2 (Toll Bridge Seismic Retrofit Program - Cost Summary) on page 4 and Table 3 (Toll Bridge Seismic Retrofit Program - Schedule Summary) on page 5 to reflect the cost and schedule issues related to China. Correspondingly, provide an updated explanation on these issues in the Risk Management Program Updates on page 24.<ul style="list-style-type: none">➢ Considering these issues have been around since early this year, the TBPOC agreed that the light should be changed to yellow in the report. This could be changed back to green next quarter should the situation improve.○ The Chair noted that the pictures in the report are informative and helpful in showing the progress being made. | <ul style="list-style-type: none">• The TBPOC delegated authority to the PMT to APPROVE the Second Quarter Report, June 30, 2008.• Make the report modifications as discussed.<ul style="list-style-type: none">➢ The Department to provide the TBPOC the Risk Management SAS explanation on page 24 by COB Monday, August 4, for review.➢ The TBPOC to respond by noon on Wednesday, August 6.• Include an update on China issues (ABF proposal, design presence, etc.) in the September 4 meeting agenda. |
| <p>2. OTHER BUSINESS</p> <ul style="list-style-type: none">• TBPOC China Visit<ul style="list-style-type: none">○ The visit scheduled for the week of | |

(continued)

| Items | Action |
|-------------------------------------------------------------------------------------------------------------------------------------------------------------------------|--------|
| September 29 was summarized. The TBPOC meeting is set for Wednesday, October 1st. ➤ The Department is making arrangements to have ABF represented at the meeting. | |

Adjourned: 2:10 PM

CONFERENCE CALL MINUTES

July 31, 2008, 2:00 PM – 2:30 PM

APPROVED BY:

WILL KEMPTON, Director
California Department of Transportation

Date

JOHN F. BARNA, Jr., Executive Director
California Transportation Commission

Date

STEVE HEMINGER, Executive Director
Bay Area Toll Authority

Date

Memorandum

TO: Toll Bridge Program Oversight Committee (TBPOC) **DATE:** August 27, 2008

FR: Andrew Fremier, Deputy Executive Director, BATA

RE: Agenda No. - 2c, d

Item- Consent Calendar
Revised 2008 TBPOC Meeting Calendar
2009 TBPOC Meeting Calendar

Recommendation:

APPROVAL

Cost:

N/A

Schedule Impacts:

N/A

Discussion:

The PMT requests approval of the attached 2008 TBPOC Meeting Calendar which was revised to show the meeting in China on October 1, and the proposed 2009 TBPOC Meeting Calendar.

Attachments:

2008 TBPOC Meeting Calendar (as of August 27, 2008)

2009 TBPOC Meeting Calendar (as of August 27, 2008)

ITEM 2: CONSENT CALENDAR

c. 2008 TBPOC Meeting Calendar

2008 TBPOC Meeting Calendar
(as of August 27, 2008)

Revised 08/27/08

| JANUARY 2008 | | | | |
|--------------|---------|---------|-----|-----|
| MON | TUE | WED | THU | FRI |
| | HOLIDAY | 1 | 2 | 3 |
| | | BATA OC | CTC | 4 |
| PMT | 7 | 8 | 9 | 10 |
| PMT | 14 | 15 | 16 | 17 |
| HOLIDAY | 21 | 22 | 23 | 24 |
| PMT | 28 | 29 | 30 | 31 |

1 - New Years Day Observed
21 - M L King Jr's Birthday

| FEBRUARY 2008 | | | | |
|---------------|-----|-----|-----|-----|
| MON | TUE | WED | THU | FRI |
| | | | | 1 |
| PMT | 4 | 5 | 6 | 7 |
| PMT | 11 | 12 | 13 | 14 |
| HOLIDAY | 18 | 19 | 20 | 21 |
| PMT | 25 | 26 | 27 | 28 |

12 - Lincoln's Birthday
18 - Washington's Birthday

| MARCH 2008 | | | | |
|------------|-----|-----|-----|-----|
| MON | TUE | WED | THU | FRI |
| | | | | |
| PMT | 3 | 4 | 5 | 6 |
| PMT | 10 | 11 | 12 | 13 |
| PMT | 17 | 18 | 19 | 20 |
| PMT | 24 | 25 | 26 | 27 |
| HOLIDAY | 31 | | | |

31 - Cesar Chavez's Birthday

| APRIL 2008 | | | | |
|------------|-----|-----|-------|-----|
| MON | TUE | WED | THU | FRI |
| | 1 | 2 | TBPOC | 4 |
| PMT | 7 | 8 | 9 | 10 |
| PMT | 14 | 15 | 16 | 17 |
| PMT | 21 | 22 | 23 | 24 |
| PMT | 28 | 29 | 30 | |

| MAY 2008 | | | | |
|----------|-----|-----|-----|-------|
| MON | TUE | WED | THU | FRI |
| | | | 1 | TBPOC |
| PMT | 5 | 6 | 7 | 8 |
| PMT | 12 | 13 | 14 | 15 |
| CHN | 19 | 20 | 21 | 22 |
| HOLIDAY | 26 | 27 | 28 | 29 |

26 - Memorial Day

| JUNE 2008 | | | | |
|-----------|-----|-----|-----|-----|
| MON | TUE | WED | THU | FRI |
| PMT | 2 | 3 | 4 | 5 |
| PMT | 9 | 10 | 11 | 12 |
| PMT | 16 | 17 | 18 | 19 |
| PMT | 23 | 24 | 25 | 26 |
| CST | 30 | | | |

| JULY 2008 | | | | |
|-----------|-----|-----|-----|---------|
| MON | TUE | WED | THU | FRI |
| | 1 | 2 | 3 | HOLIDAY |
| PMT | 7 | 8 | 9 | 10 |
| PMT | 14 | 15 | 16 | 17 |
| PMT | 21 | 22 | 23 | 24 |
| PMT | 28 | 29 | 30 | 31 |

4 - Independence Day

| AUGUST 2008 | | | | |
|-------------|-----|-----|-----|-----|
| MON | TUE | WED | THU | FRI |
| | | | | 1 |
| PMT | 4 | 5 | 6 | 7 |
| 2 Final | 11 | 12 | 13 | 14 |
| PMT | 18 | 19 | 20 | 21 |
| RM | 25 | 26 | 27 | 28 |

| SEPTEMBER 2008 | | | | |
|----------------|-----|-----|-----|-----|
| MON | TUE | WED | THU | FRI |
| HOLIDAY | 1 | 2 | 3 | 4 |
| PMT | 8 | 9 | 10 | 11 |
| PMT | 15 | 16 | 17 | 18 |
| CST | 22 | 23 | 24 | 25 |
| CHN | 29 | 30 | | |

1 - Labor Day

| OCTOBER 2008 | | | | |
|--------------|-----|-------|-----|-----|
| MON | TUE | WED | THU | FRI |
| | | TBPOC | 1 | 2 |
| PMT | 6 | 7 | 8 | 9 |
| HOLIDAY | 13 | 14 | 15 | 16 |
| PMT | 20 | 21 | 22 | 23 |
| PMT | 27 | 28 | 29 | 30 |

13 - Columbus Day

| NOVEMBER 2008 | | | | |
|---------------|-----|-----|-----|-----|
| MON | TUE | WED | THU | FRI |
| PMT | 3 | 4 | 5 | 6 |
| PMT | 10 | 11 | 12 | 13 |
| PMT | 17 | 18 | 19 | 20 |
| RM | 24 | 25 | 26 | 27 |

11 - Veteran's Day
27, 28 - Thanksgiving Day and day after

| DECEMBER 2008 | | | | |
|---------------|-----|-----|-----|-----|
| MON | TUE | WED | THU | FRI |
| PMT | 1 | 2 | 3 | 4 |
| PMT | 8 | 9 | 10 | 11 |
| PMT | 15 | 16 | 17 | 18 |
| PMT | 22 | 23 | 24 | 25 |
| CST | 29 | 30 | 31 | |

25 - Christmas Day observed

| | |
|-------|----------------------------------------|
| | Qtrly Rept Schedule |
| Final | TBPOC Final Comments |
| Leg | Issue to Legislature & CTC |
| RM | Risk Management Briefing to PMT |
| CST | Corridor Schedule Team Briefing to PMT |

PMT Meetings in Oakland, 1:00 PM - 2:30 PM
TBPOC Meetings in Sacramento, 1:00 PM - 4:00 PM
TBPOC Mtgs in Bay Area, 10:00 AM - 1:00 PM

ITEM 2: CONSENT CALENDAR

d. 2009 TBPOC Meeting Calendar

2009 TBPOC Meeting Calendar
(as of August 27, 2008)

| Jan-09 | | | | |
|---------|--------|---------|---------|-----|
| MON | TUE | WED | THU | FRI |
| | | | HOLIDAY | |
| | | | 1 | 2 |
| PMT | | CTC | CTC | |
| 5 | 6 | 7 | 8 | 9 |
| PMT | TBPOC | BATA OC | | |
| 12 | Bay 13 | 14 | 15 | 16 |
| HOLIDAY | PMT | | | |
| 19 | 20 | 21 | 22 | 23 |
| PMT | | MTG | | |
| 26 | 27 | 28 | 29 | 30 |

1 - New Years Day Observed
19 - M L King Jr's Birthday

| Feb-09 | | | | |
|---------|---------|-------|---------|-----|
| MON | TUE | WED | THU | FRI |
| | | TBPOC | | |
| 2 | 3 | Bay 4 | 5 | 6 |
| PMT | 4 Final | 4 Leg | HOLIDAY | |
| 9 | 10 | 11 | 12 | 13 |
| Holiday | PMT | CTC | CTC | |
| 16 | 17 | 18 | 19 | 20 |
| PMT | | MTG | | |
| 23 | 24 | 25 | 26 | 27 |
| PMT | | | | |
| 29 | 26 | 27 | 28 | 29 |

12 - Lincoln's Birthday
16 - Washington's Birthday

| Mar-09 | | | | |
|--------|---------|---------|--------|-----|
| MON | TUE | WED | THU | FRI |
| PMT | | | Leg Up | |
| 2 | 3 | 4 | TBPOC | |
| | | | Sac 5 | 6 |
| PMT | | BATA OC | CTC | |
| 9 | 10 | 11 | 12 | 13 |
| PMT | | CTC | | |
| 16 | 17 | 18 | 19 | 20 |
| PMT | | MTG | | |
| 23 | 24 | 25 | 26 | 27 |
| PMT | HOLIDAY | | | |
| 30 | 31 | | | |

31 - Cesar Chavez's Birthday

| Apr-09 | | | | |
|--------|-----|---------|-------|-----|
| MON | TUE | WED | THU | FRI |
| | | | TBPOC | |
| | | 1 | 2 | 3 |
| PMT | | BATA OC | CTC | |
| 6 | 7 | 8 | 9 | 10 |
| PMT | | | | |
| 13 | 14 | 15 | 16 | 17 |
| PMT | | MTG | | |
| 20 | 21 | 22 | 23 | 24 |
| PMT | | | | |
| 27 | 28 | 29 | 30 | |

| May-09 | | | | |
|---------|-----|---------|-------|---------|
| MON | TUE | WED | THU | FRI |
| | | | | 1 |
| PMT | | | TBPOC | 1 Final |
| 4 | 5 | 6 | Bay 7 | 8 |
| 1 Leg | | BATA OC | CTC | |
| PMT | | CTC | CTC | |
| 11 | 12 | 13 | 14 | 15 |
| PMT | | | | |
| 18 | 19 | 20 | 21 | 22 |
| HOLIDAY | PMT | MTG | | |
| 25 | 26 | 27 | 28 | 29 |

25 - Memorial Day

| Jun-09 | | | | |
|--------|-----|---------|-------|-----|
| MON | TUE | WED | THU | FRI |
| PMT | | | TBPOC | |
| 1 | 2 | 3 | Sac 4 | 5 |
| PMT | | BATA OC | CTC | |
| 8 | 9 | 10 | 11 | 12 |
| PMT | | | | |
| 15 | 16 | 17 | 18 | 19 |
| PMT | | MTG | | |
| 22 | 23 | 24 | 25 | 26 |
| PMT | | | | |
| 29 | 30 | | | |

| Jul-09 | | | | |
|--------|-----|---------|-------|---------|
| MON | TUE | WED | THU | FRI |
| | | | TBPOC | HOLIDAY |
| | | 1 | Bay 2 | 3 |
| PMT | | BATA OC | CTC | |
| 6 | 7 | 8 | 9 | 10 |
| PMT | | | | |
| 13 | 14 | 15 | 16 | 17 |
| PMT | | MTG | | |
| 20 | 21 | 22 | 23 | 24 |
| PMT | | | | |
| 27 | 28 | 29 | 30 | 31 |

3 - Day before Independence Day

| Aug-09 | | | | |
|--------|-----|---------|-------|-----|
| MON | TUE | WED | THU | FRI |
| PMT | | | TBPOC | |
| 3 | 4 | 5 | Bay 6 | 7 |
| PMT | | 2 Final | 2 Leg | |
| 11 | 12 | 13 | 14 | 15 |
| PMT | | | | |
| 17 | 18 | 19 | 20 | 21 |
| PMT | | | | |
| 24 | 25 | 26 | 27 | 28 |
| PMT | | | | |
| 31 | | | | |

| Sep-09 | | | | |
|---------|-----|---------|-------|-----|
| MON | TUE | WED | THU | FRI |
| | | | TBPOC | |
| | 1 | 2 | Sac 3 | 4 |
| HOLIDAY | PMT | BATA OC | CTC | |
| 7 | 8 | 9 | 10 | 11 |
| PMT | | | | |
| 14 | 15 | 16 | 17 | 18 |
| PMT | | MTG | | |
| 21 | 22 | 23 | 24 | 25 |
| PMT | | | | |
| 28 | 29 | 30 | | |

7 - Labor Day

| Oct-09 | | | | |
|---------|-----|---------|-------|-----|
| MON | TUE | WED | THU | FRI |
| | | | TBPOC | |
| | | | 1 | 2 |
| PMT | | CTC | CTC | |
| 5 | 6 | 7 | 8 | 9 |
| HOLIDAY | PMT | BATA OC | | |
| 12 | 13 | 14 | 15 | 16 |
| PMT | | | | |
| 19 | 20 | 21 | 22 | 23 |
| PMT | | MTG | | |
| 26 | 27 | 28 | 29 | 30 |

12 - Columbus Day

| Nov-09 | | | | |
|--------|---------|---------|---------|---------|
| MON | TUE | WED | THU | FRI |
| PMT | | | TBPOC | |
| 2 | 3 | 4 | Bay 5 | 6 |
| PMT | 3 Final | 3 Leg | HOLIDAY | |
| 9 | 10 | 11 | 12 | 13 |
| PMT | | BATA OC | CTC | |
| 16 | 17 | 18 | 19 | 20 |
| PMT | | MTG | HOLIDAY | HOLIDAY |
| 23 | 24 | 25 | 26 | 27 |
| PMT | | | | |
| 30 | | | | |

11 - Veteran's Day
26, 27 - Thanksgiving Day and day after

| Dec-09 | | | | |
|--------|-----|---------|-------|---------|
| MON | TUE | WED | THU | FRI |
| | | | TBPOC | |
| | 1 | 2 | Sac 3 | 4 |
| PMT | | BATA OC | CTC | |
| 7 | 8 | 9 | 10 | 11 |
| PMT | | MTG | | |
| 14 | 15 | 16 | 17 | 18 |
| PMT | | | | HOLIDAY |
| 21 | 22 | 23 | 24 | 25 |
| PMT | | | | |
| 28 | 29 | 30 | 31 | |

25 - Christmas Day observed

| | |
|-------|----------------------|
| | Qtrly Rept Schedule |
| Final | TBPOC Final Comments |
| Leg | Issue to Legislature |

PMT Meetings in Oakland, 1:00 PM - 2:30 PM
TBPOC Meetings in Sacramento, 1:00 PM - 4:00 PM
TBPOC Meetings in the Bay Area, 10:00 AM - 1:00 PM

ITEM 3: PROGRESS REPORTS

- a. Draft August 2008 Monthly Progress Report

Memorandum

TO: Toll Bridge Program Oversight Committee (TBPOC) **DATE:** August 27, 2008

FR: Andrew Fremier, Deputy Executive Director, BATA

RE: Agenda No. - 3a
Progress Reports
Item- Draft August 2008 Monthly Progress Report

Recommendation:

For Information Only / Approval Confirmation

Cost:

N/A

Schedule Impacts:

N/A

Discussion:

The PMT approved the final July 2008 Monthly Progress Report and the Second Quarter Report, June 30, 2008 through delegated TBPOC authority on August 11, 2008, and requests TBPOC confirmation of these approvals.

In the back of this packet is a draft August 2008 Monthly Progress Report, for your information. TBPOC approval of this report, through PMT delegation, is anticipated as soon as updated expenditure data and final comments are incorporated.

Enclosure:

Draft August 2008 Monthly Progress Report



Toll Bridge Seismic Retrofit and Regional Measure 1 Programs

Monthly Progress Report August 2008

Draft
Version 3.0



TOLL BRIDGE PROGRAM
OVERSIGHT COMMITTEE

CALTRANS BAY AREA TOLL AUTHORITY CALIFORNIA TRANSPORTATION COMMISSION

Released: September 2008



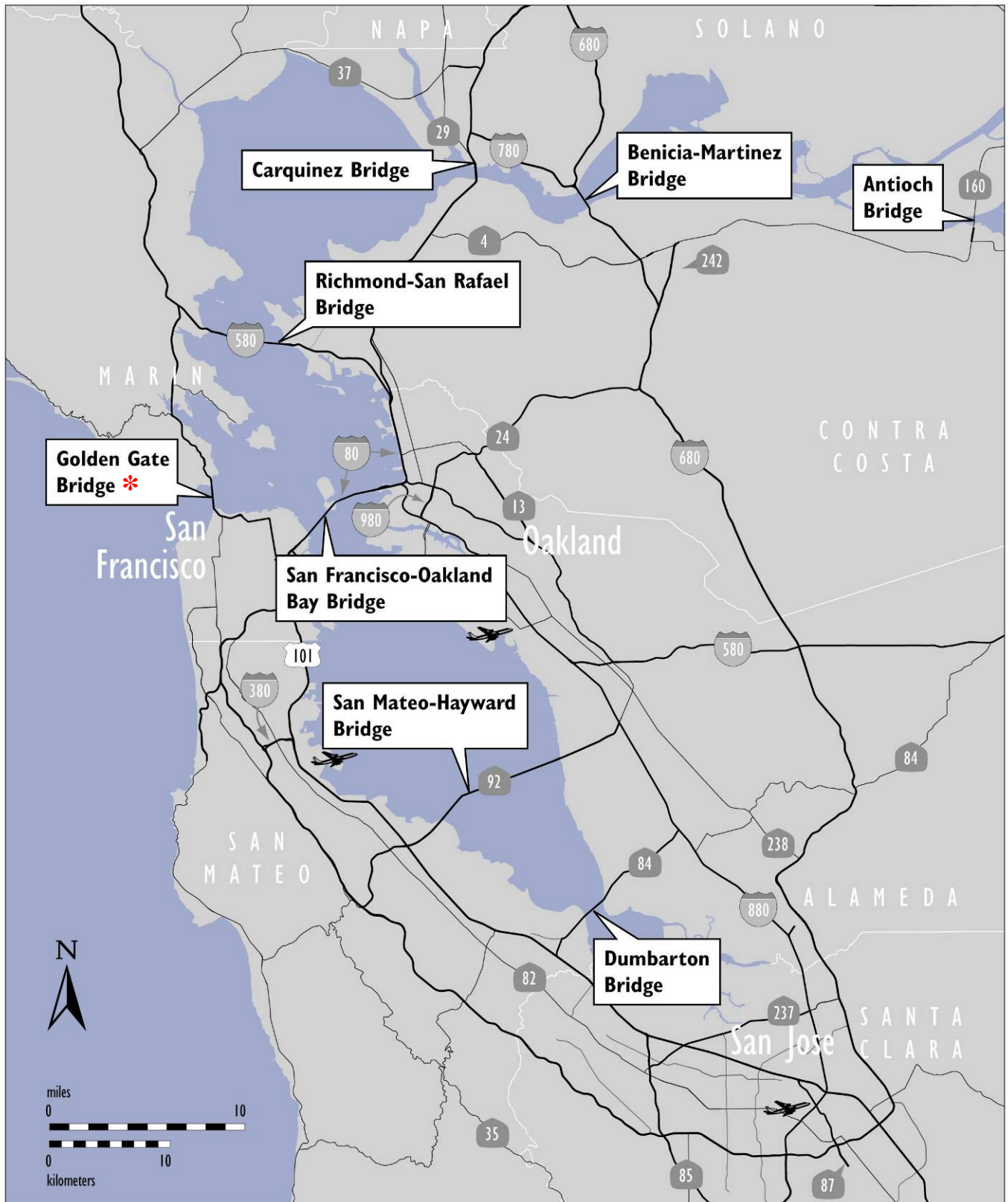
Toll Bridge Seismic Retrofit and Regional Measure 1 Programs

Monthly Progress Report
August 2008

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Toll Bridges of the San Francisco Bay Area



* Under the Jurisdiction of the Golden Gate Bridge, Highway and Transportation District
 Note: Details may not sum to totals due to rounding effects.

INTRODUCTION

In July 2005, Assembly Bill 144, (AB 144) Hancock created the Toll Bridge Project Oversight Committee (TBPOC) to implement a project oversight and project control process for the Benicia-Martinez Bridge project and the state toll bridge seismic retrofit program projects. Comprising the Caltrans' Director, the Bay Area Toll Authority (BATA) Executive Director and the Executive Director of the California Transportation Commission (CTC), the TBPOC's project oversight and control processes include, but are not limited to, reviewing bid specifications and documents, providing field staff to review ongoing costs, reviewing and approving significant change orders and claims in excess of \$1 million (as defined by the committee) and preparing project reports.

AB 144 identified the Toll Bridge Seismic Retrofit Program and the new Benicia-Martinez Bridge Project as being under the direct oversight of the TBPOC. The Toll Bridge Seismic Retrofit Program includes:

| Toll Bridge Seismic Retrofit Projects | Seismic Safety Status |
|-------------------------------------------------------------|-----------------------|
| San Francisco-Oakland Bay Bridge East Span Replacement | Construction |
| San Francisco-Oakland Bay Bridge West Approach Replacement | Construction |
| San Francisco-Oakland Bay Bridge West Span Seismic Retrofit | Complete |
| San Mateo-Hayward Bridge Seismic Retrofit | Complete |
| Richmond-San Rafael Bridge Seismic Retrofit | Complete |
| Eastbound Carquinez Bridge Seismic Retrofit | Complete |
| New Benicia-Martinez Bridge Seismic Retrofit | Complete |
| San Diego-Coronado Bridge Seismic Retrofit | Complete |
| Vincent Thomas Bridge Seismic Retrofit | Complete |

The new Benicia-Martinez Bridge is part of a larger program of toll-funded projects, called the Regional Measure 1 (RM1) Toll Bridge Program, under the responsibility of the BATA. While the rest of the projects in the RM1 program are not directly under the responsibility of the TBPOC, BATA and Caltrans (CT) will continue to report on their progress as an informational item. The RM1 program includes:

| RM1 Projects | Open to Traffic Status |
|------------------------------------------------------------------------|------------------------|
| Interstate 880/State Route 92 Interchange Reconstruction | Construction |
| New Benicia-Martinez Bridge | Open |
| Richmond-San Rafael Bridge Deck Overlay Rehabilitation | Open |
| Richmond-San Rafael Bridge Trestle, Fender & Deck Joint Rehabilitation | Open |
| Westbound Carquinez Bridge Replacement | Open |
| San Mateo-Hayward Bridge Widening | Open |
| State Route 84 Bayfront Expressway Widening | Open |
| Richmond Parkway | Open |

This report focuses on identifying critical project issues and monitoring project cost and schedule performance for the projects as measured against approved budgets and schedule milestones. This report is intended to fulfill Caltrans' requirement to provide monthly project progress reporting to the TBPOC under Section 30952.05 of the Streets and Highway Code.

EXECUTIVE SUMMARY

Toll Bridge Seismic Retrofit Program—Cost (\$ Millions)

| Project | Work Status | AB 144 / SB 66 Budget (07/20/05) | Approved Changes | Current Approved Budget (07/2008) | Cost To Date (06/2008) | Cost Forecast* | At- Completion Variance | Cost Status |
|---------------------------------------------------|--------------|-------------------------------------------|---------------------|--------------------------------------------|------------------------------|-------------------|-------------------------------|-------------|
| a | b | c | d | e = c + d | f | g | h = g - e | i |
| SFOBB East Span Replacement Project | | | | | | | | |
| Capital Outlay Support | | 959.3 | - | 959.3 | 620.3 | 977.1 | 17.8 | ● |
| Capital Outlay Construction | | | | | | | | |
| Skyway | Complete | 1,293.0 | (38.9) | 1,254.1 | 1,234.1 | 1,254.1 | - | ● |
| SAS E2/T1 Foundations | Complete | 313.5 | (32.6) | 280.9 | 272.7 | 280.9 | - | ● |
| SAS Superstructure | Construction | 1,753.7 | - | 1,753.7 | 466.0 | 1,767.4 | 13.7 | ● |
| YBI Detour | Design/Const | 132.0 | 310.2 | 442.2 | 191.4 | 461.2 | 19.0 | ● |
| YBI Transition Structures | | 299.3 | (23.2) | 276.1 | - | 276.1 | - | ● |
| * YBITS Contract No. 1 | Design | | | | - | 214.3 | | |
| * YBITS Contract No. 2 | Design | | | | - | 58.5 | | |
| * YBITS Contract No. 3 - Landscape | Design | | | | - | 3.3 | | |
| Oakland Touchdown (OTD) | | 283.8 | - | 283.8 | 102.4 | 302.5 | 18.7 | |
| * OTD Submarine Cable | Complete | | | | 7.9 | 9.6 | | ● |
| * OTD No. 1 (Westbound) | Construction | | | | 94.5 | 226.5 | | ● |
| * OTD No. 2 (Eastbound) | Design | | | | - | 62.0 | | ● |
| * OTD Electrical Systems | Design | | | | - | 4.4 | | ● |
| Existing Bridge Demolition | Design | 239.2 | - | 239.2 | - | 222.0 | (17.2) | ● |
| Stormwater Treatment Measures | Complete | 15.0 | 3.3 | 18.3 | 16.4 | 18.3 | - | ● |
| East Span Completed Projects | | 90.3 | - | 90.3 | 89.2 | 90.3 | - | |
| Right-of-Way and Environmental Mitigation | | 72.4 | - | 72.4 | 39.3 | 72.4 | - | ● |
| Other Budgeted Capital | | 35.1 | (3.3) | 31.8 | 0.7 | 7.7 | (24.1) | |
| Total SFOBB East Span Replacement Project | | 5,486.6 | 215.5 | 5,702.1 | 3,032.5 | 5,730.0 | 27.9 | |
| SFOBB West Approach Replacement | Construction | | | | | | | ● |
| Capital Outlay Support | | 120.0 | - | 120.0 | 107.4 | 120.0 | - | |
| Capital Outlay Construction | | 309.0 | 24.7 | 333.7 | 282.9 | 350.7 | 17.0 | ● |
| Total SFOBB West Approach Replacement | | 429.0 | 24.7 | 453.7 | 390.3 | 470.7 | 17.0 | |
| Richmond-San Rafael Bridge Retrofit | Complete | | | | | | | ● |
| Capital Outlay Support | | 134.0 | (7.0) | 127.0 | 126.7 | 127.0 | - | |
| Capital Outlay Construction & Right-of-Way | | 780.0 | (90.5) | 689.5 | 666.6 | 689.5 | - | |
| Total Richmond-San Rafael Bridge Retrofit | | 914.0 | (97.5) | 816.5 | 793.3 | 816.5 | - | |
| Program Completed Projects | Complete | | | | | | | |
| Capital Outlay Support | | 219.8 | - | 219.8 | 219.4 | 219.8 | - | |
| Capital Outlay Construction | | 705.6 | - | 705.6 | 698.1 | 705.6 | - | |
| Total Program Completed Projects | | 925.4 | - | 925.4 | 917.5 | 925.4 | - | |
| Miscellaneous Program Costs | | 30.0 | - | 30.0 | 24.7 | 30.0 | - | |
| Program Contingency | | 900.0 | (142.7) | 757.3 | - | 712.4 | 44.9 | |
| Total Toll Bridge Seismic Retrofit Program | | 8,685.0 | - | 8,685.0 | 5,158.3 | 8,685.0 | - | |

● Within Approved Current Schedule and Budget

● Potential Cost and Schedule Impacts: Possible future need for Program Contingency Allocation

● Known Cost and Schedule Impacts: Request for Program Contingency Allocation forthcoming

*Current contract allotment to install two submarine electrical cables is \$11.5 million. Additional non-program funding to support this allocation beyond the \$9.6 million of available program funds has been made available by the Treasure Island Development Authority.

Notes: Details may not sum to totals due to rounding effects.

Forecasts for the Monthly Reports are generally updated on a quarterly basis in conjunction with Risk Analysis assessments for the TBSRP Projects and the TBSRP Quarterly Reports.

Toll Bridge Seismic Retrofit Program—Schedule

| Project | AB 144 / SB 66 Project Complete Baseline (07/2005) | Approved Changes (Months) | Project Complete Current Approved Schedule (07/2008) | Project Complete Schedule Forecast (07/2008) | Schedule Variance (Months) | Schedule Status | Remarks |
|-------------------------------------------------|-------------------------------------------------------------------|---------------------------------|---------------------------------------------------------------------|----------------------------------------------------------|----------------------------------|--------------------|-----------------------------------------------------------------------------------------------------------------------------------------------------|
| a | b | c | d = b + c | e | f = e - d | g | h |
| SFOBB East Span Replacement Project | | | | | | | |
| Skyway | Apr 07 | 8 | Dec 07 | Dec 07 | - | ● | See page 10. |
| SAS E2/T1 Foundations | Jun 08 | (3) | Mar 08 | Jan 08 | (2) | ● | |
| SAS Superstructure | Mar 12 | 12 | Mar 13 | Mar 13 | - | ● | See Note. |
| YBI Detour | Jul 07 | 36 | Jun 10 | Jun 10 | - | ● | See discussion on pages 16 and 17. |
| YBI Transition Structures | Nov 13 | 12 | Nov 14 | Nov 14 | - | ● | |
| Oakland Touchdown (OTD) | Nov 13 | 12 | Nov 14 | Nov 14 | - | ● | See Note. |
| • OTD Submarine Cable | n/a | | Jan 08 | Jan 08 | - | ● | |
| • OTD Westbound | n/a | | Jan 10 | Jan 10 | - | ● | |
| • OTD Eastbound | n/a | | Nov 14 | Nov 14 | - | ● | |
| Existing Bridge Demolition | Sep 14 | 12 | Sep 15 | Sep 15 | - | ● | See Note. |
| Stormwater Treatment Measures | Mar 08 | - | Mar 08 | Mar 08 | - | ● | |
| ◆ Open to Traffic Date: Westbound | Sep 11 | 12 | Sep 12 | Sep 12 | - | ● | See Note. |
| ◆ Open to Traffic Date: Eastbound | Sep 12 | 12 | Sep 13 | Sep 13 | - | ● | See Note. |
| SFOBB West Approach Replacement | Aug 09 | - | Aug 09 | Jan 09 | (7) | ● | |
| ◆ Open to Traffic Date: Mainline Realignment | n/a | - | Apr 08 | Apr 08 | - | ● | Opened to traffic April 12, 2008 |
| Richmond-San Rafael Bridge | | | | | | | |
| • Seismic Retrofit | Aug 05 | - | Aug 05 | Oct 05 | 2 | ● | Seismic retrofit completed July 29, 2005. Formal acceptance of contract October 28, 2005. \$89 million has been transferred to Program Contingency. |
| • Public Access Project | n/a | - | May 07 | Sept 07 | 4 | ● | See page 34. |

Note: Schedules for selected projects and the Open to Traffic dates were extended by 12 months from the AB144/SB66 baseline schedule due to Addenda #5 and #7 on the SAS Superstructure contract.

Regional Measure 1 Program—Cost (\$ Millions)

| Project | Work Status | BATA Budget (07/2005) | Approved Changes | Current Approved Budget (07/2008) | Cost To Date (07/2008) | Cost Forecast* | At- Completion Variance | Cost Status |
|-----------------------------------------------------|--------------|-----------------------------|---------------------|--------------------------------------------|------------------------------|-------------------|-------------------------------|-------------|
| a | b | c | d | e = c + d | f | g | h = g - e | i |
| New Benicia-Martinez Bridge Project | Construction | | | | | | | ● |
| Capital Outlay Support | | 157.1 | 35.2 | 192.3 | 182.6 | 192.3 | - | |
| Capital Outlay Construction | | 861.6 | 173.5 | 1,035.1 | 967.2 | 1,035.1 | - | |
| Capital Outlay Right-of-Way | | 20.4 | (0.1) | 20.3 | 16.9 | 20.3 | - | |
| Project Reserve | | 20.8 | 4.0 | 24.8 | - | 24.8 | - | |
| Total New Benicia-Martinez Bridge Project | | 1,059.9 | 212.6 | 1,272.5 | 1,166.7 | 1,272.5 | - | |
| Carquinez Bridge Replacement Project | Complete | | | | | | | ● |
| Capital Outlay Support | | 124.4 | (0.2) | 124.2 | 123.5 | 123.6 | (0.6) | |
| Capital Outlay Construction | | 381.2 | 3.2 | 384.4 | 376.1 | 384.5 | 0.1 | |
| Capital Outlay Right-of-Way | | 10.5 | - | 10.5 | 9.9 | 10.5 | - | |
| Project Reserve | | 12.1 | (3.0) | 9.1 | - | 0.6 | (8.5) | |
| Total Carquinez Bridge Replacement Project | | 528.2 | - | 528.2 | 509.5 | 519.2 | (9.0) | |
| I-880/SR-92 Interchange Reconstruction | Construction | | | | | | | ● |
| Capital Outlay Support | | 28.8 | 26.2 | 55.0 | 40.8 | 55.0 | - | |
| Capital Outlay Construction | | 94.8 | 60.2 | 155.0 | 29.6 | 155.0 | - | |
| Capital Outlay Right-of-Way | | 9.9 | 7.0 | 16.9 | 11.0 | 16.9 | - | |
| Project Reserve | | 0.3 | 17.8 | 18.1 | - | 18.1 | - | |
| Total I-880/SR-92 Interchange Reconstruction | | 133.8 | 111.2 | 245.0 | 81.4 | 245.0 | - | |
| Program Completed Projects | Complete | | | | | | | |
| Capital Outlay Support | | 62.0 | (5.0) | 57.0 | 57.5 | 58.8 | 1.8 | |
| Capital Outlay Construction | | 324.4 | 3.6 | 328.0 | 308.0 | 313.0 | (15.0) | |
| Capital Outlay Right-of-Way | | 1.7 | - | 1.7 | 0.5 | 0.8 | (0.9) | |
| Project Reserve | | 2.6 | 1.4 | 4.0 | - | 7.1 | 3.1 | |
| Total Program Completed Projects | | 390.7 | - | 390.7 | 366.0 | 379.7 | (11.0) | |
| Total Regional Measure 1 Program | | 2,112.6 | 323.8 | 2,436.4 | 2,123.6 | 2,416.4 | (20.0) | |

- Within Approved Current Schedule and Budget
- Potential Cost and Schedule Impacts: Possible future need for Program Contingency Allocation
- Known Cost and Schedule Impacts: Request for Program Contingency Allocation forthcoming

Note: Details may not sum to totals due to rounding effects.

Forecasts for the Monthly Reports are generally updated on a quarterly basis in conjunction with Risk Analysis assessments for the TBSRP Projects and the TBSRP Quarterly Reports.

Regional Measure 1 Program—Schedule

| Project | BATA Project Complete Baseline (07/2005) | Approved Changes (Months) | Project Complete Current Approved Schedule (07/2008) | Project Complete Schedule Forecast (07/2008) | Schedule Variance (Months) | Schedule Status | Remarks |
|-----------------------------------------------|------------------------------------------------------|---------------------------------|---------------------------------------------------------------------|----------------------------------------------------------|----------------------------------|--------------------|--------------------------------------------------------------------------------|
| a | b | c | d = b + c | e | f = e - d | g | h |
| New Benicia-Martinez Bridge Project | | | | | | | |
| • Existing Bridge & Interchange Modifications | Dec 09 | - | Dec 09 | Dec 09 | - | ● | |
| • I-680/I-780 Interchange Replacement | Dec 07 | - | Dec 07 | Dec 07 | - | ● | |
| • Open to Traffic Date | Dec 07 | - | Aug 07 | Aug 07 | (3) | ● | |
| I-880/SR-92 Interchange Reconstruction | Dec 10 | - | Jun 11 | Jun 11 | 6 | ● | Contract was awarded on August 28, 2007 with the approval of the State budget. |

Highlights of Project/Program Activities and TBPOC Actions for July 2008

Toll Bridge Seismic Retrofit Program

SFOBB East Span Seismic Replacement Project

- ◆ On the Yerba Buena Island Detour Contract, BATA allocated a TBPOC recommended budget supplement to the contract on June 25 to cover construction risks and to provide additional contract contingency. The budget supplement was funded from project savings from the Skyway, E2/T1, and Richmond-San Rafael contracts and from the program contingency. Work is progressing on the detour viaduct structure just south of the existing bridge.

New Benicia-Martinez Bridge Project

- ◆ On the Existing Bridge Modification Project, southbound Interstate 680 traffic is scheduled to be shifted from the west to east side of the bridge over the weekend of August 15th. Follow-on work includes reconstruction of the west side bridge deck, addition of the pedestrian/bicycle lane, and to repair roadway undulations.

Interstate 880/State Route 92 Interchange Reconstruction Project

- ◆ Interstate 880 is scheduled to be closed intermittently at night over the week of August 26th to erect falsework for the main eastbound 92 to northbound 880 connector.



Barge Fabrication in China



PROJECT / CONTRACT REPORTS

Toll Bridge Seismic Retrofit Program

San Francisco-Oakland Bay Bridge (SFOBB) East Span Replacement Project Summary

- Skyway Contract
- Self-Anchored Suspension (SAS) E2/T1 Foundations Contract
- Self-Anchored Suspension (SAS) Superstructure Contract
- Yerba Buena Island (YBI)
 - Yerba Buena Island (YBI) Detour Contract
 - Yerba Buena Island (YBI) Transition Structure Contracts
- Oakland Touchdown (OTD)
 - Oakland Touchdown (OTD) Submarine Cable Relocation Contract
 - Oakland Touchdown (OTD) #1 Contract
 - Oakland Touchdown (OTD) #2 Contract
- Other Major Contracts
- Other Contracts and Related Project Work

San Francisco-Oakland Bay Bridge (SFOBB) West Approach Replacement Project

Richmond-San Rafael Bridge Seismic Retrofit Project

Other Completed Seismic Retrofit Projects

Toll Bridge Seismic Retrofit Program

San Francisco-Oakland Bay Bridge (SFOBB) East Span Replacement Project Summary

Project Description: The East Span will be seismically retrofitted through the complete replacement of the existing span. The remaining effort for this project consists of the following contracts: SAS Superstructure—construction of a self-anchored 385-meter main span superstructure incorporating a 160-meter fabricated structural steel tower with a main cable and inclined suspenders that will support steel orthotropic decks; Yerba Buena Island (YBI) Detour—design and construction of a temporary double-deck bypass structure that will detour traffic to the existing SFOBB while completing the westerly permanent tie-in structure of the new East Span at Yerba Buena Island; YBI Structures—construction of a new structure connecting the western end of the self-anchored suspension to the Yerba Buena Island viaduct, which will be retrofitted; Oakland Touchdown—at the Oakland end of the East Span, construction of two parallel, cast-in-place post-tensioned concrete viaducts, which join the Skyway to the at-grade Oakland approach fill; and Existing Bridge Demolition—demolition of the existing 1936 SFOBB East Span structure after the construction and placement of traffic onto the new East Span.

SFOBB East Span Replacement Cost Summary (\$ Millions)

| Contract | AB 144/ SB 66 Budget | Approved Changes | Current Approved Budget | Cost To Date (06/2008) | Cost Forecast (07/2008) | Variance |
|----------------------------------------------|-------------------------|---------------------|-------------------------------|------------------------------|-------------------------------|-------------|
| a | b | c | d = b + c | e | f | g = f - d |
| Capital Outlay Support | 959.3 | - | 959.3 | 620.3 | 977.1 | 17.8 |
| Capital Outlay | - | - | - | - | - | - |
| Skyway | 1,293.0 | (38.9) | 1,254.1 | 1,234.1 | 1,254.1 | - |
| SAS E2/T1 Foundations | 313.5 | (32.6) | 280.9 | 272.7 | 280.9 | - |
| SAS Superstructure | 1,753.7 | - | 1,753.7 | 466.0 | 1,767.4 | 13.7 |
| YBI Detour | 132.0 | 310.2 | 442.2 | 191.4 | 461.2 | 19.0 |
| YBI Transition Structures | 299.3 | (23.2) | 276.1 | - | 276.1 | - |
| * YBITS 1 | | | | - | 214.3 | |
| * YBITS 2 | | | | - | 58.5 | |
| * YBITS 3 - Landscape | | | | - | 3.3 | |
| Oakland Touchdown | 283.8 | - | 283.8 | 102.4 | 302.5 | 18.7 |
| * OTD Submarine Cable | | | | 7.9 | 9.6 | |
| * OTD Westbound | | | | 94.5 | 226.5 | |
| * OTD Eastbound | | | | - | 62.0 | |
| * OTD Electrical Systems | | | | - | 4.4 | |
| Existing Bridge Demolition | 239.2 | - | 239.2 | - | 222.0 | (17.2) |
| Stormwater Treatment Measures | 15.0 | 3.3 | 18.3 | 16.4 | 18.3 | - |
| East Span Completed Projects | 90.3 | - | 90.3 | 89.2 | 90.3 | - |
| Right-of-Way and Environmental Mitigation | 72.4 | - | 72.4 | 39.3 | 72.4 | - |
| Other Budgeted Capital | 35.1 | (3.3) | 31.8 | 0.7 | 7.7 | (24.1) |
| TOTAL | 5,486.6 | 215.5 | 5,702.1 | 3,032.5 | 5,730.0 | 27.9 |

SFOBB East Span Replacement Schedule Summary

| Contract | AB 144/SB 66 Contract Completion Baseline (07/2005) | Approved Changes (Months) | Contract Complete Current Approved Schedule (07/2008) | Contract Complete Schedule Forecast (07/2008) | Schedule Variance (Months) |
|-------------------------------|-----------------------------------------------------------------|---------------------------------|----------------------------------------------------------------------|-----------------------------------------------------------|----------------------------------|
| Skyway | April 2007 | 8 | December 2007 | December 2007 | - |
| YBI Detour* | July 2007 | 36 | June 2010 | June 2010 | - |
| Stormwater Treatment Measures | March 2008 | - | March 2008 | March 2008 | - |
| SAS E2/T1 Foundations | June 2008 | (3) | March 2008 | March 2008 | - |
| SAS Superstructure | March 2012 | 12 | March 2013 | March 2013 | - |
| Oakland Touchdown (OTD) | November 2013 | 12 | December 2014 | December 2014 | - |
| * OTD Submarine Cable | n/a | | January 2008 | January 2008 | - |
| * OTD No. 1 (Westbound) | n/a | | January 2010 | January 2010 | - |
| * OTD No. 2 (Eastbound) | n/a | | November 2014 | November 2014 | - |
| YBI Transition Structure* | November 2013 | 12 | November 2014 | November 2014 | - |
| Existing Bridge Demolition* | September 2014 | 12 | September 2015 | September 2015 | - |
| Open to Traffic: Westbound | September 2011 | 12 | September 2012 | September 2012 | - |
| Open to Traffic: Eastbound | September 2012 | 12 | September 2013 | September 2013 | - |

*Contract schedules being further assessed due to changes in SAS schedule.

Project Status: Construction is complete for the Skyway, SAS E2/T1 Foundations and Stormwater Treatment Measures contracts. Construction is currently ongoing for the YBI Detour, SAS Superstructure, and OTD #1 (Westbound) contracts. Contracts in design include the OTD #2 (eastbound), the YBI Transition Structure (YBITS) Contract #1, YBITS Contract #2 and the Existing Bridge Demolition contract. Design of each contract is proceeding per its schedule requirements.

Project Issues: All projects except Demolition have a Risk Response Team and a Risk Register incorporating quantitative risk analyses. A risk register has also been developed for Capital Outlay Support (COS) costs, as well as a program-level risk register that captures risks common to all project. The development of a quantitative COS risk analysis is ongoing and is trending higher COS costs for the project.

The Risk Response Team for COS is evaluating the program costs and is developing response actions to mitigate. Many of the actions have been effective, as evidenced by a reduction of risk impacts on the Skyway and E2/T1 contracts from the previous quarter. The effort to develop and execute risk response actions to mitigate the cost and schedule impacts posed by risk issues continues to be a high priority.

Recent TBPOC Actions: See the following contract detail pages for specific TBPOC actions on East Span contracts.

Toll Bridge Seismic Retrofit Program

San Francisco-Oakland Bay Bridge (SFOBB) East Span Replacement Project

► SKYWAY CONTRACT

Contract Description: On the SFOBB East Span Seismic Replacement Project, the Skyway contract constructed twin pre-cast concrete segmental bridges that will connect the Oakland approach traffic to the new SAS.

Skyway Cost Summary (\$ Millions)

| Contract | AB 144 / SB 66 Budget (07/2005) | Approved Changes | Current Approved Budget (07/2008) | Cost To Date (06/2008) | Cost Forecast (07/2008) | Variance |
|-----------------------------|------------------------------------------|---------------------|--------------------------------------------|---------------------------|-------------------------------|-----------|
| a | b | c | d = b + c | e | f | g = f - d |
| East Span - Skyway | | | | | | |
| Capital Outlay Support | 197.0 | (16.0) | 181.0 | 180.1 | 181.0 | - |
| Capital Outlay Construction | 1,293.0 | (38.9) | 1,254.1 | 1,234.1 | 1,254.1 | - |
| TOTAL | 1,490.0 | (54.9) | 1,435.1 | 1,414.2 | 1,435.1 | - |

Note: Details may not sum to totals due to rounding effects.

Skyway Schedule Summary

| Contract | AB 144/SB 66 Contract Completion Baseline (07/2005) | Approved Changes (Months) | Contract Complete Current Approved Schedule (07/2008) | Contract Complete Schedule Forecast (07/2008) | Schedule Variance (Months) |
|-----------------------|--------------------------------------------------------------|---------------------------------|-------------------------------------------------------------------|--------------------------------------------------------|----------------------------------|
| East Span - Skyway | April 2007 | 8 | December 2007 | December 2007 | - |

Contract Status: The contract was substantially completed by the end of 2007 and Caltrans accepted the Skyway Contract on March 24, 2008 upon completion of final punchlist items. The TBPOC is forecasting that the \$1,293.0 million Skyway contract will be closed-out with \$38.9 million in project savings that can be returned to the program contingency.

Contract Issues: None.

Recent TBPOC Actions: None.

Toll Bridge Seismic Retrofit Program

San Francisco-Oakland Bay Bridge (SFOBB) East Span Replacement Project

► SELF-ANCHORED SUSPENSION (SAS) E2/T1 FOUNDATIONS CONTRACT

Contract Description: The Self Anchored Suspension (SAS) Span E2/T1 Foundation contract constructed the main tower foundation at location T1 and the foundations and columns of the first pier east of the main tower at location E2 in San Francisco Bay. The foundations and columns of the first pier west of the main tower located at W2 on Yerba Buena Island were completed under a separate earlier contract.

SAS E2/T1 Foundations Cost Summary (\$ Millions)

| Contract | AB 144 / SB 66 Budget (07/2005) | Approved Changes | Current Approved Budget (07/2008) | Cost To Date (06/2008) | Cost Forecast (07/2008) | Variance |
|----------------------------------------|------------------------------------------|---------------------|--------------------------------------------|---------------------------|-------------------------------|-----------|
| a | b | c | d = b + c | e | f | g = f - d |
| East Span - SAS E2 / T1 Foundations | | | | | | |
| Capital Outlay Support | 52.5 | (21.5) | 31.0 | 28.1 | 31.0 | - |
| Capital Outlay Construction | 313.5 | (32.6) | 280.9 | 272.7 | 280.9 | - |
| TOTAL | 366.0 | (54.1) | 311.9 | 300.8 | 311.9 | - |

Note: Details may not sum to totals due to rounding effects.

SAS E2/T1 Foundations Schedule Summary

| Contract | AB 144/SB 66 Contract Completion Baseline (07/2005) | Approved Changes (Months) | Contract Complete Current Approved Schedule (07/2008) | Contract Complete Schedule Forecast (07/2008) | Schedule Variance (Months) |
|----------------------------------------|--------------------------------------------------------------|---------------------------------|-------------------------------------------------------------------|--------------------------------------------------------|----------------------------------|
| East Span - SAS E2 / T1 Foundations | June 2008 | (3) | March 2008 | January 2008 | (2) |

Contract Status: The SAS E2/T1 Marine Foundations Contract was completed and accepted by Caltrans on January 18, 2008. With completion of this contract, all foundations for the SAS have now been completed.

The TBPOC is forecasting that the \$313.5 million E2/T1 contract will be closed out with \$32.6 million in forecasted savings that can be returned to the program contingency.

Recent TBPOC Actions: None.

Contract Photographs



Installation Progress on Barge



Barge Fabrication and Installation

Toll Bridge Seismic Retrofit Program

San Francisco-Oakland Bay Bridge (SFOBB) East Span Replacement Project

► SELF-ANCHORED SUSPENSION (SAS) SUPERSTRUCTURE CONTRACT

Contract Description: The Self-Anchored Suspension (SAS) Superstructure contract constructs a signature tower span between the Skyway and the Yerba Buena Island transition structure. Work on the SAS bridge has been split between three contracts—the SAS Superstructure (under construction), the SAS E2/T1 Foundation (completed), and the SAS W2 Foundation (completed).

SAS Superstructure Cost Summary (\$ Millions)

| Contract | AB 144 / SB 66 Budget (07/2005) | Approved Changes | Current Approved Budget (07/2008) | Cost To Date (06/2008) | Cost Forecast (07/2008) | Variance |
|--------------------------------|------------------------------------------|---------------------|--------------------------------------------|---------------------------|-------------------------------|-------------|
| a | b | c | d = b + c | e | f | g = f - d |
| East Span - SAS Superstructure | | | | - | | |
| Capital Outlay Support | 214.6 | - | 214.6 | 91.9 | 214.6 | - |
| Capital Outlay Construction | 1,753.7 | - | 1,753.7 | 466.0 | 1,767.4 | 13.7 |
| TOTAL | 1,968.3 | - | 1,968.3 | 557.9 | 1,982.0 | 13.7 |

Note: Details may not sum to totals due to rounding effects.

SAS Superstructure Schedule Summary

| Contract | AB 144/SB 66 Contract Completion Baseline (07/2005) | Approved Changes (Months) | Contract Complete Current Approved Schedule (07/2008) | Contract Complete Schedule Forecast (07/2008) | Schedule Variance (Months) |
|--------------------------------|--------------------------------------------------------------|---------------------------------|-------------------------------------------------------------------|--------------------------------------------------------|----------------------------------|
| East Span - SAS Superstructure | March 2012 | 12 | March 2013 | March 2013 | - |

Contract Status: The contract is 32% complete as of July, 2008 based on expended value of the contract. The contractor, American Bridge Flour Enterprises, Inc., a joint venture (ABF) and their subcontractors continue to prepare and submit Requests For Information (RFIs), drawing submittals, procedures and schedule update for Caltrans review and response.

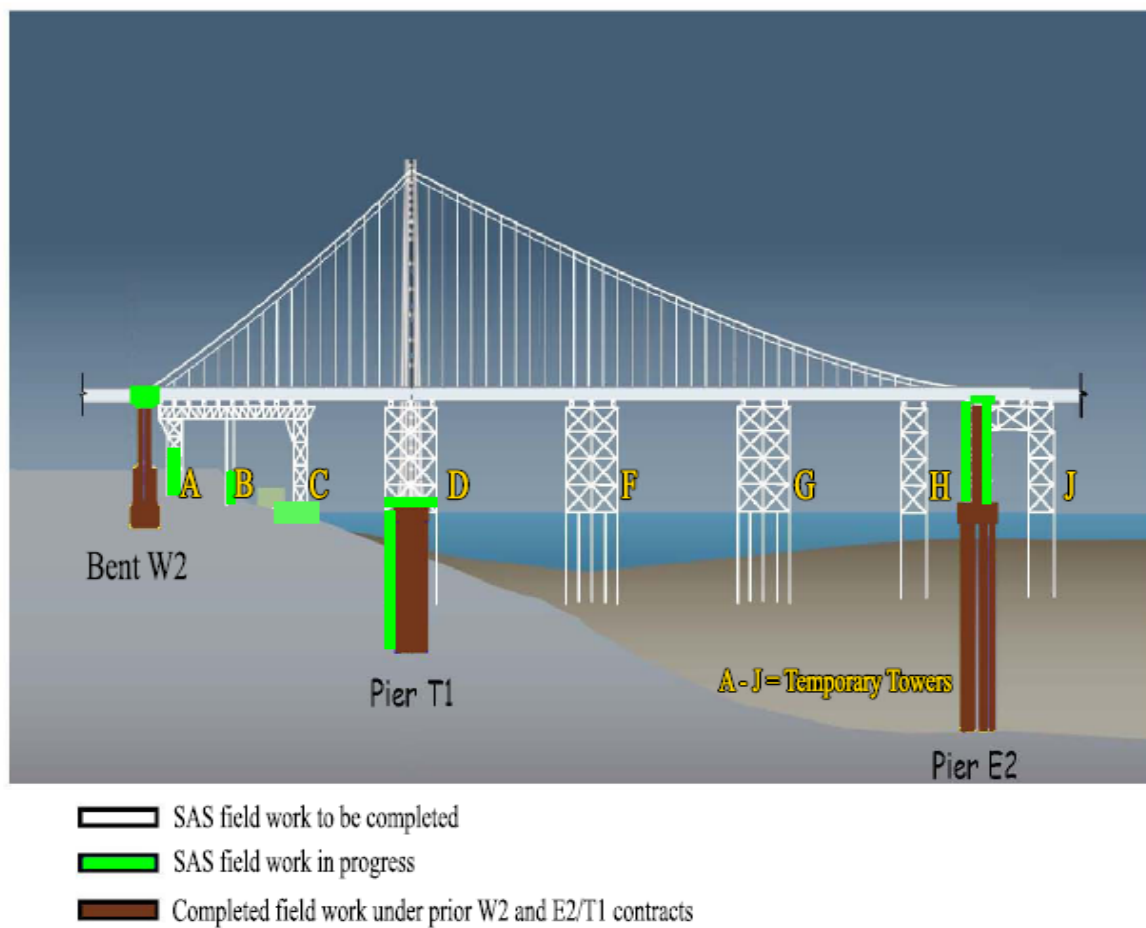
The Shearleg Barge Crane fabrication continues and is expected to be completed and shipped to the Bay Area in the fall of 2008. Temporary towers "A" and "B" foundations have been completed and temporary tower "C" foundation started in July and continues in support of CIDH. Structural steel fabrication for the temporary tower frames, truss members, beams, driving frames and piles continue. The temporary towers "A" and "B" tower frame erection started in July and continues. Temporary tower "D" pile driving frame installation is completed and pile driving continues. E2 crossbeam falsework and W2 cap beam reinforcing steel and embed installation continue. The third concrete placement for W2 is scheduled in late August. Hinge "K" fabrication continues. OBG fabrication of plates and segment assembly for lifts one through eight continues. T1 partial mock-up fabrications continue, and are forecast for completion in August. The T1 tower lift one shaft skin plates, diaphragm fabrication and fit up and welding of flanges continues. Tower saddle fabrication and W2 cap beam deviation saddle fabrication continue at JSW in Japan. The cable system hand rope and utility stanchion procurement was completed in July 2008 and fabrication started this August.

Contract Issues:

| Issue | Mitigating Action |
|------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|
| The SAS contractor has stated that the fabrication schedule for the Orthotropic Box Girder (OBG) is 4-5 months behind schedule. While not yet on the critical path for the project, this delay may increase and result in additional cross-impacts to the corridor schedule. | The contractor and the Department are developing opportunities for acceleration to mitigate this delay. |
| Potential for cost increases during construction due to steel plate conflicts. Applies to structural steel, including the towers and box girders. | Establish Working Drawing Campus with Contractor to facilitate discussion about conflicts and meet regularly. Caltrans has constructed models and identified conflicts, for which CCOs are to be prepared. |

Recent TBPOC Actions: None

SAS Superstructure Construction Progress



Contract Photographs from Changxing Island, China



Boom Completed and Third Coat Has Been Applied



Tower Fabrication Shop

Toll Bridge Seismic Retrofit Program

San Francisco-Oakland Bay Bridge (SFOBB) East Span Replacement Project

► YERBA BUENA ISLAND DETOUR (YBID)

● YBI DETOUR CONTRACT

Contract Description: The YBI Detour constructs a temporary detour from the YBI tunnel to the existing east span of the Bay Bridge. This detour maintains traffic on the existing bridge while the YBI Transition Structure Contract completes the tie-in from the SAS to the existing tunnel.

YBI Detour Cost Summary (\$ Millions)

| Contract | AB 144 / SB 66 Budget (07/2005) | Approved Changes | Current Approved Budget (07/2008) | Cost To Date (06/2008) | Cost Forecast (07/2008) | Variance |
|-----------------------------|------------------------------------------|---------------------|--------------------------------------------|------------------------------|-------------------------------|-------------|
| a | b | c | d = b + c | e | f | g = f - d |
| YBI Detour | | | | | | |
| Capital Outlay Support | 29.4 | 36.6 | 66.0 | 44.7 | 66.0 | - |
| Capital Outlay Construction | 132.0 | 310.2 | 442.2 | 191.4 | 461.2 | 19.0 |
| TOTAL | 161.4 | 346.8 | 508.2 | 236.1 | 527.2 | 19.0 |

Note: Details may not sum to totals due to rounding effects.

YBI Detour Schedule Summary

| Contract | AB 144/SB 66 Contract Completion Baseline (07/2005) | Approved Changes (Months) | Contract Complete Current Approved Schedule (07/2010) | Contract Complete Schedule Forecast (07/2010) | Schedule Variance (Months) |
|--------------|--------------------------------------------------------------|---------------------------------|-------------------------------------------------------------------|-----------------------------------------------------------|----------------------------------|
| YBI Detour * | July 2005 | 40 | June 2010 | June 2010 | - |

* Contract schedule under assessment. See Contract Issues on the following page.

Contract Status: The YBI Detour Contract was awarded in early 2004 to construct a temporary detour structure providing for, at that time, a new bridge opening in 2006. Due to the re-advertisement of the SAS superstructure contract in 2005, the bridge opening was rescheduled to 2013, which necessitated a temporary suspension of the YBI Detour contract and design changes. The required suspension of work and design revisions have resulted in increased cost for the YBI Detour contract.

In 2006, the TBPOC approved a plan to pace work on the project, to have Caltrans assume design responsibility over the east and west tie-ins, and to make changes to the detour structures to allow it to stand in place alone for a longer duration than originally intended. The YBI Detour contract is now forecast to be completed in 2010, consistent with the planned westbound opening date of 2012 for the new bridge.

In addition to the revised contract completion date, the TBPOC approved on February 15, 2007 to advance foundation and retrofit work from the Yerba Buena Island Transition Structures (YBITS) contract to the YBI Detour contract. Advancing the work will reduce overall project schedule risk by taking work off the critical path for the East Span project while making more effective use of the extended YBI Detour contract duration, and will enable potential acceleration of the SAS construction pending negotiation with American Bridge.

Significant construction risks have been identified that will require additional funds to be budgeted for the project. In March 2008, the TBPOC approved a revised forecast of \$461.2 million for the project with additional contingencies to cover the risks and has redirected project savings from the E2/T1, Skyway, and Richmond-San Rafael Bridge contracts

and TBSRP program contingency to cover the increases. In June 2008, the TBPOC approved a revised budget of \$442.2 million for the contract.

Fabrication of the temporary detour viaduct was completed in Pohang, Korea and all steel members have arrived. Viaduct bent caps construction is complete and steel erection for the viaduct is ongoing. The contractor continued to install rebar for the lower and upper deck of span 48 of the viaduct, and is progressing with the steel erection for span 49. Span 50 falsework erection also continues.

CCOs were issued for the procurement of the tower legs and raw steel materials and steel fabrication is ongoing on the skid bents and truss for the East Tie-in section of the detour. Construction is currently in progress for the skid bent foundations piles to support the roll out/roll in section of the East Tie-in. Lead paint removal from the existing bridge in support of RORI continues.

CCO approval process is on going for the following CCOs:

- ❖ CCO # 140 – ETI Truss Fabrication (Revision)
- ❖ CCO # 141 – WTI Phase 2 Superstructure
- ❖ CCO # 129 – Skid Bents and Truss Erection
- ❖ CCO # 65 – Bridge Demolition

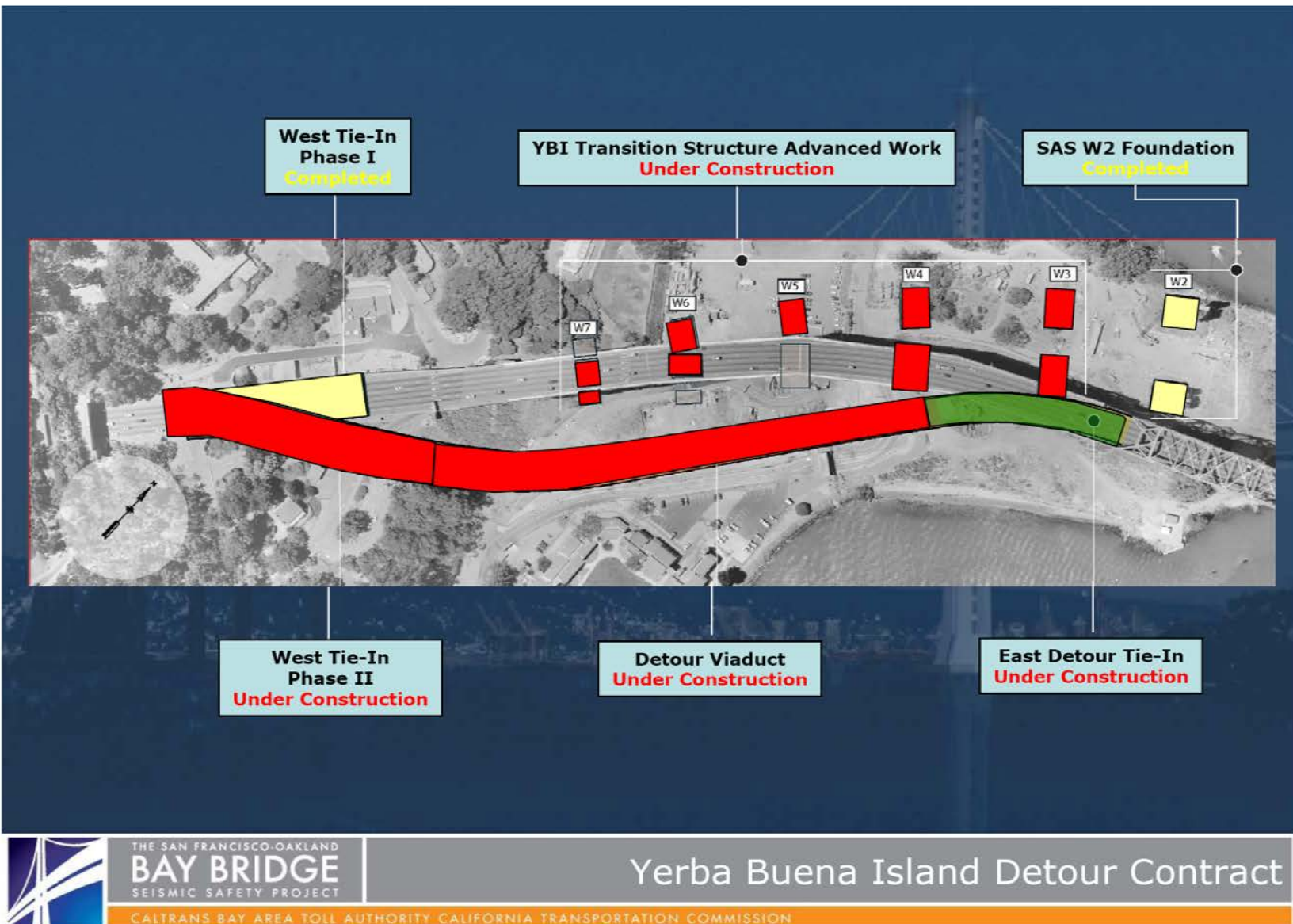
Foundation and column work for the West Tie-in Phase 2 Substructure contract is in progress.

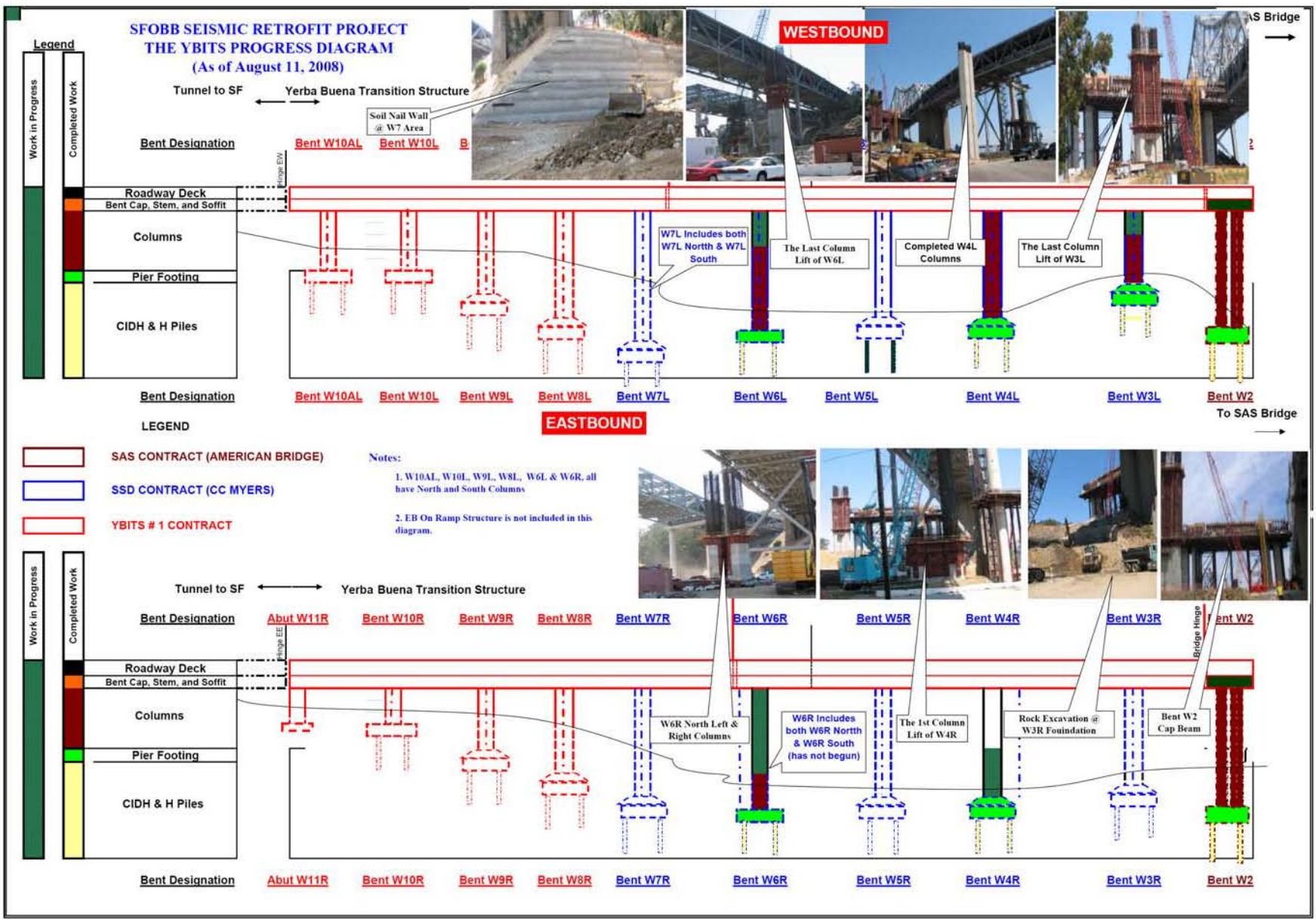
As part of the YBITS Advanced Work, the contractor is continuing on the W7 area soil nail wall, final lift of the column at W6L, and the second lift of the W6R-N column. W4L column construction is complete, while the W4L column work continues. Rebar installation for the last lift of W3L and the foundation excavation for W3R continue.

Recent TBPOC Actions: On the Yerba Buena Island Detour Contract, BATA allocated a TBPOC recommended budget supplement to the contract on June 25 to cover construction risks and to provide additional contract contingency. The budget supplement was funded from project savings from the Skyway, E2/T1, and Richmond-San Rafael contracts and from the program contingency. CCO # 140 – ETI Truss Fabrication (Revision) is scheduled for TBPOC approval in their September 2008 scheduled meeting.

Contract Issues:

| Issue | Mitigating Action |
|-----------------------------------------------------------------------------------------------------------------------------|-----------------------------------------------------------------------------------------------------------------------------------|
| Caltrans will need to negotiate a number of contract change orders to implement the aforementioned changes to the contract. | The TBPOC has approved a plan of action to implement the changes. Caltrans is currently negotiating outstanding contract changes. |





Notes:

- W10AL, W10L, W9L, W8L, W6L & W6R, all have North and South Columns
- EB On Ramp Structure is not included in this diagram.

Contract Photographs



Viaduct Bent 5, Span 50



Viaduct Span 48 Lower Deck Concrete

Contract Photographs (cont.)

Existing Bridge Lead Abatement for RORI



Viaduct Span 48 Upper Deck

Toll Bridge Seismic Retrofit Program

San Francisco-Oakland Bay Bridge (SFOBB) East Span Replacement Project

► YBI TRANSITION CONTRACTS (YBITS)

Contract Description: The YBI Transition Structure contracts will construct the mainline YBI transition structures (YBITS) that will connect the SAS portion of the new bridge to the newly rolled in WTI Phase I structure. YBITS #1 will construct the mainline approach structure from the new bridge to the WTI Phase I structure. YBITS #2 will demolish the YBI Detour temporary structure, complete the new eastbound on-ramp, reconstruct local affected facilities at YBI and complete the bike path from the SAS to YBI (except for a section of the path that conflicts with existing column E1). That section of the path is contemplated to be completed in the demolition contract. A YBI landscaping contract will restore slopes and vegetation in areas affected by the YBI construction.

YBI Transition Structure Cost Summary (\$ Millions)

| Contract | AB 144 / SB 66 Budget (07/2005) | Approved Changes | Current Approved Budget (07/2008) | Cost To Date (06/2008) | Cost Forecast (07/2008) | Variance |
|-----------------------------------|------------------------------------------|---------------------|--------------------------------------------|------------------------------|-------------------------------|-----------|
| a | b | c | d = b + c | e | f | g = f - d |
| Capital Outlay Support | 78.7 | - | 78.7 | 20.3 | 78.7 | - |
| Capital Outlay Construction | | | | | | |
| * YBITS Contract #1 | | | | - | 214.3 | |
| * YBITS Contract #2 | | | | - | 58.5 | |
| * YBITS Contract #3 - Landscape | | | | - | 3.3 | |
| Total Capital Outlay Construction | 299.3 | (23.2) | 276.1 | - | 276.1 | - |
| TOTAL | 378.0 | (23.2) | 354.8 | 20.3 | 354.8 | - |

Note: Details may not sum to totals due to rounding effects.

YBI Transition Structure Schedule Summary

| Contract | AB 144/SB 66 Contract Completion Baseline (06/2005) | Approved Changes (Months) | Contract Complete Current Approved Schedule (07/2008) | Contract Complete Schedule Forecast (07/2008) | Schedule Variance (Months) |
|--------------------------|--------------------------------------------------------------|---------------------------------|-------------------------------------------------------------------|-----------------------------------------------------------|----------------------------------|
| YBI Transition Structure | November 2013 | 12 | November 2014 | November 2014 | - |

Contract Status: In February 2007, the TBPOC approved a plan to accelerate portions of the YBITS work by adding it to the YBI Detour contract. The new forecast for the YBITS contract excluding the advanced work is \$276.1 million, which is a net reduction of \$23.2 million from the AB 144/SB 66 budget. Caltrans advertised the project on August 11, 2008.

Contract Issues: None.

Recent TBPOC Actions: None.

Contract Photographs



YBITS W4L



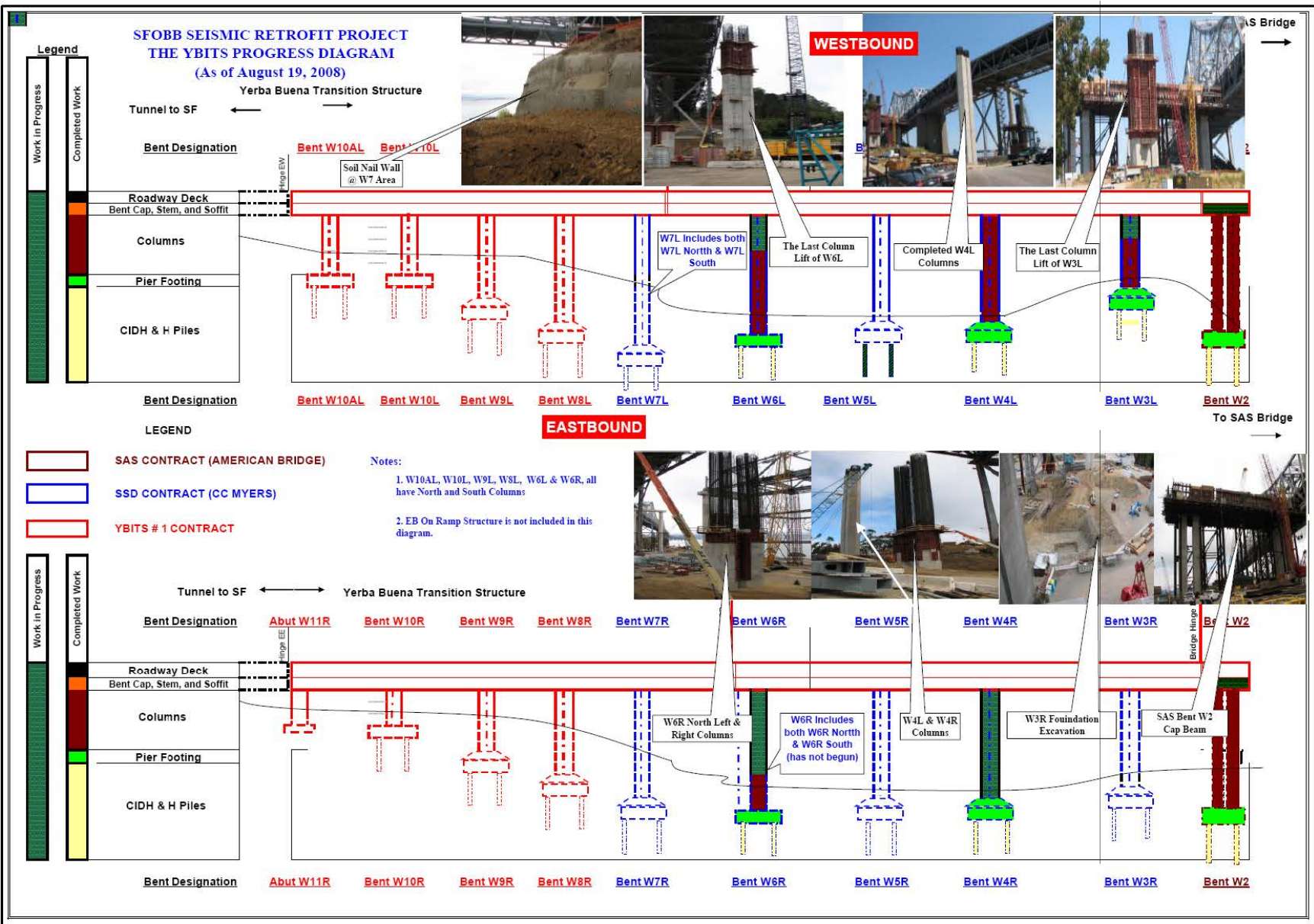
YBITS W4L



YBITS W4R Column



Viaduct Span 48



Toll Bridge Seismic Retrofit Program

San Francisco-Oakland Bay Bridge (SFOBB) East Span Replacement Project

► OAKLAND TOUCHDOWN CONTRACTS

Contract Descriptions: The Oakland Touchdown #1 Contract includes construction of all marine foundations, and land foundations (except for the eastbound abutment), westbound bridge section, and one frame of the eastbound bridge section and roadway approach for the section connecting the new Skyway portion to the roadway west of the Oakland Toll Plaza.

The Oakland Touchdown #2 Contract includes construction of the remaining eastbound bridge section and roadway approach for the section connecting the new Skyway portion to the roadway west of the Oakland Toll Plaza. This work would occur once the westbound traffic is shifted onto the new westbound bridge, including the SAS.

The Submarine Cable Relocation Contract replaced the existing submarine electrical cable from Oakland to Treasure Island and was completed ahead of the OTD Contract #1 which avoided potential construction conflicts.

Oakland Touchdown Cost Summary (\$ Millions)

| Contract | AB 144 / SB 66 Budget (07/2005) | Approved Changes | Current Approved Budget (07/2008) | Cost To Date (06/2008) | Cost Forecast (07/2008) | Variance |
|-----------------------------------|------------------------------------------|---------------------|--------------------------------------------|------------------------------|-------------------------------|-------------|
| a | b | c | d = b + c | e | f | g = f - d |
| Capital Outlay Support | 74.4 | - | 74.4 | 38.0 | 92.1 | 17.7 |
| Capital Outlay Construction | | | | | | |
| OTD Submarine Cable | - | - | - | 7.9 | 9.6 | - |
| Oakland Touchdown #1 | - | - | - | 94.5 | 226.5 | - |
| Oakland Touchdown #2 | - | - | - | - | 62.0 | - |
| Oakland Touchdown Electrical | - | - | - | - | 4.4 | - |
| Total Capital Outlay Construction | 283.8 | - | 283.8 | 102.4 | 303.5 | 18.7 |
| TOTAL | 358.2 | - | 358.2 | 140.4 | 394.6 | 36.4 |

Note: Details may not sum to totals due to rounding effects. The allocation of AB144/SB 66 budgets is proceeding. Budget amount is TBD. Overall OTD budgets and forecasts are shown on page 2.

Oakland Touchdown Schedule Summary

| Contract | AB 144/SB 66 Contract Completion Baseline (6/2005) | Approved Changes (Months) | Contract Complete Current Approved Schedule (07/2008) | Contract Complete Schedule Forecast (07/2008) | Schedule Variance (Months) |
|----------------------|----------------------------------------------------------------|---------------------------------|-------------------------------------------------------------------|-----------------------------------------------------------|----------------------------------|
| OTD Submarine Cable | - | - | January 2008 | January 2008 | - |
| Oakland Touchdown #1 | - | - | January 2010 | January 2010 | - |
| Oakland Touchdown #2 | - | - | November 2014 | November 2014 | - |

Contract Status

Oakland Touchdown Contract #1: The project is approximately 51% complete based on expended value of the contract as of July 30, 2008. The Department continued to review and process various contractors' RFIs and submittals. Work on the substructure has been completed for the WB structure, while the superstructure for the WB structure and the substructure work for the eastbound bridge are both in progress. The detailed progress status of the project can be viewed on the updated OTD1 progress diagram as of August 13, 2008, as shown on page 27.

Submarine Cable Relocation Contract: All field work has been completed and the contractor has demobilized. Caltrans has accepted the contract.

Contract Issues: None.

Recent TBPOC Actions: None.





E17R – Footing Rebar Installation in Progress



E17 to E20L – Frame 1 Soffit Deck Erection Completed



Fit E17 to E20L – Top Deck View of WB Frame 1



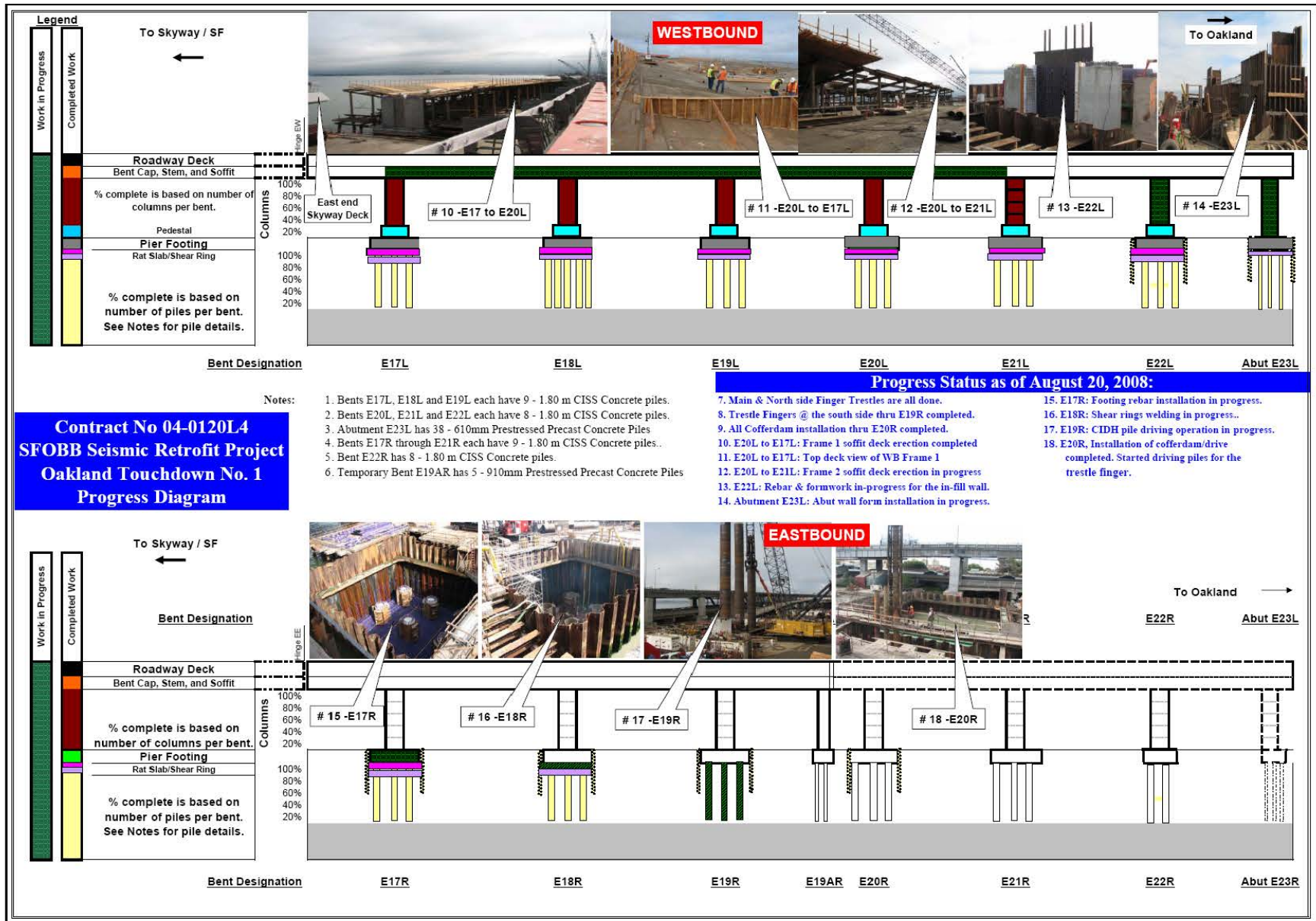
E19R – CIDH Pile Driving Operation in Progress



E22L – Rebar and Formwork in Progress for the In-Fill Wall.



Abutment E23L – Abut Wall Form Installation in Progress



Toll Bridge Seismic Retrofit Program

San Francisco-Oakland Bay Bridge (SFOBB) East Span Replacement Project

► OTHER CONTRACTS

Contract Description: Other Major Contracts include the Stormwater Treatment Measures contract, which will implement best practices for storm water runoff treatment at the SFOBB toll plaza and approaches to the SFOBB toll plaza and the Existing Bridge Demolition contract, which will include the complete removal of the existing 1936 east span following the opening of the new bridge.

Other Major Contracts Cost Summary (\$ Millions)

| Contract | AB 144 / SB 66 Budget (6/2005) | Approved Changes | Current Approved Budget (07/2008) | Cost To Date (06/2008) | Cost Forecast (07/2008) | Variance |
|-----------------------------------|-----------------------------------------|---------------------|--------------------------------------------|---------------------------|-------------------------------|---------------|
| a | b | c | d = b + c | e | f | g = f - d |
| Capital Outlay Support | 85.7 | 2.0 | 87.7 | 8.2 | 87.7 | - |
| Capital Outlay Construction | | | | | | - |
| Existing Bridge Demolition | 239.2 | - | 239.2 | - | 222.0 | (17.2) |
| Stormwater Treatment Measures | 15.0 | 3.3 | 18.3 | 16.4 | 18.3 | - |
| Total Capital Outlay Construction | 254.2 | 3.3 | 257.5 | 16.4 | 240.3 | (17.2) |
| TOTAL | 339.9 | 5.3 | 345.2 | 24.6 | 328.0 | (17.2) |

Note: Details may not sum to totals due to rounding effects.

Other Major Contracts Schedule Summary

| Contract | AB 144/SB 66 Contract Completion Baseline (07/2005) | Approved Changes (Months) | Contract Complete Current Approved Schedule (07/2008) | Contract Complete Schedule Forecast (07/2008) | Schedule Variance (Months) | % Design Comp. |
|-------------------------------|-----------------------------------------------------------------|---------------------------------|-------------------------------------------------------------------|--------------------------------------------------------|----------------------------------|----------------------|
| Existing Bridge Demolition | September 2014 | 12 | September 2015 | September 2015 | - | 10 |
| Stormwater Treatment Measures | March 2008 | - | March 2008 | March 2008 | - | N/A |

Contract Status:

Stormwater Treatment Measures: The contract was accepted in December 2007.

Bridge Demolition: Design work has been temporarily suspended to assign engineering resources to higher priority tasks, and will resume at a later time. The contract schedule completion date has been extended by 12 months due to a 12-month SAS contract extension. The \$17.2 million decrease in construction costs for the Existing Bridge Demolition contract is due to a re-evaluation of cost escalation rates for the contract.

Recent TBPOC Actions: None

Toll Bridge Seismic Retrofit Program

San Francisco-Oakland Bay Bridge (SFOBB) East Span Replacement Project

► OTHER COMPLETED CONTRACTS AND RELATED WORK

Summary Description: Substantial work has already been performed on the SFOBB East Span Replacement project to facilitate construction of the mainline construction contracts.

Other Contracts and Related Work Cost Summary (\$ Millions)

| Contract | AB 144 / SB 66 Budget (07/2005) | Approved Changes | Current Approved Budget (07/2008) | Cost To Date (06/2008) | Cost Forecast (07/2008) | Variance |
|---------------------------------------------|------------------------------------------|---------------------|--------------------------------------------|------------------------------|-------------------------------|-----------|
| a | b | c | d = b + c | e | f | g = f - d |
| Capital Outlay Support | 227.0 | (1.0) | 226.0 | 209.0 | 226.0 | - |
| Right-of-Way and Environmental Mitigation | 72.4 | - | 72.4 | 39.3 | 72.4 | - |
| Capital Outlay Construction | | | | | | - |
| SAS W2 Foundations | 26.4 | - | 26.4 | 25.8 | 26.4 | - |
| YBI/SAS Archaeology | 1.1 | - | 1.1 | 1.1 | 1.1 | - |
| YBI - USCG Road Relocation | 3.0 | - | 3.0 | 2.8 | 3.0 | - |
| YBI - Substation and Viaduct | 11.6 | - | 11.6 | 11.3 | 11.6 | - |
| Oakland Geofill | 8.2 | - | 8.2 | 8.2 | 8.2 | - |
| Pile Installation Demonstration Project | 9.2 | - | 9.2 | 9.2 | 9.2 | - |
| Existing East Span Retrofit | 30.8 | - | 30.8 | 30.8 | 30.8 | - |
| Total Capital Outlay Construction Completed | 90.3 | - | 90.3 | 89.2 | 90.3 | - |
| TOTAL | 389.7 | (1.0) | 388.7 | 337.5 | 388.7 | - |

Note: Details may not sum to totals due to rounding effects.

Other Contracts and Related Work Schedule Summary

| Project | Actual Project Completion Date |
|--------------------------------------|--------------------------------|
| Existing East Span Retrofit | March 1998 |
| Interim Retrofit | July 2000 |
| Pile Installation Demolition Project | December 2000 |
| YBI / SAS Archaeology | January 2003 |
| Oakland Geofill | April 2003 |
| YBI - USCG Road Relocation | June 2004 |
| SAS W2 Foundations | October 2004 |
| YBI Substation and Viaduct | May 2005 |

Summary Status: Construction has been completed on the above-listed contracts. Caltrans continues to work with various environmental agencies to conduct compliance inspections and monitor and mitigate any environmental impacts from the project.

Contract Issues: None.

Recent TBPOC Actions: None.

Toll Bridge Seismic Retrofit Program

San Francisco-Oakland Bay Bridge (SFOBB) West Approach Replacement Project

Project Description: The SFOBB West Approach Replacement Project will replace the entire west approach structure from 5th Street to the west anchorage of the existing west spans of the SFOBB while maintaining existing traffic lanes for the weekday commute.

SFOBB West Approach Replacement Cost Summary (\$ Millions)

| Project | AB 144 / SB 66 Budget (07/2005) | Approved Changes | Current Approved Budget (07/2008) | Cost To Date (06/2008) | Cost Forecast (07/2008) | Variance |
|-----------------------------|------------------------------------------|---------------------|--------------------------------------------|---------------------------|-------------------------------|-------------|
| a | b | c | d = b + c | e | f | g = f - d |
| West Approach | | | | | | |
| Capital Outlay Support | 120.0 | - | 120.0 | 107.4 | 120.0 | - |
| Capital Outlay Construction | 309.0 | 24.7 | 333.7 | 282.9 | 350.7 | 17.0 |
| TOTAL | 429.0 | 24.7 | 453.7 | 390.3 | 470.7 | 17.0 |

Note: Details may not sum to totals due to rounding effects.

SFOBB West Approach Replacement Schedule Summary

| Project | AB 144/SB 66 Project Completion Baseline (07/2006) | Approved Changes (Months) | Project Complete Current Approved Schedule (07/2008) | Contract Complete Schedule Forecast (07/2008) | Schedule Variance (Months) |
|-----------------------------------------------|-------------------------------------------------------------|---------------------------------|---------------------------------------------------------------|-----------------------------------------------------|----------------------------------|
| West Approach | August 2009 | - | August 2009 | January 2009 | (7) |
| Open-to-Traffic Date: Mainline Realignment | | | April 2008 | April 2008 | - |

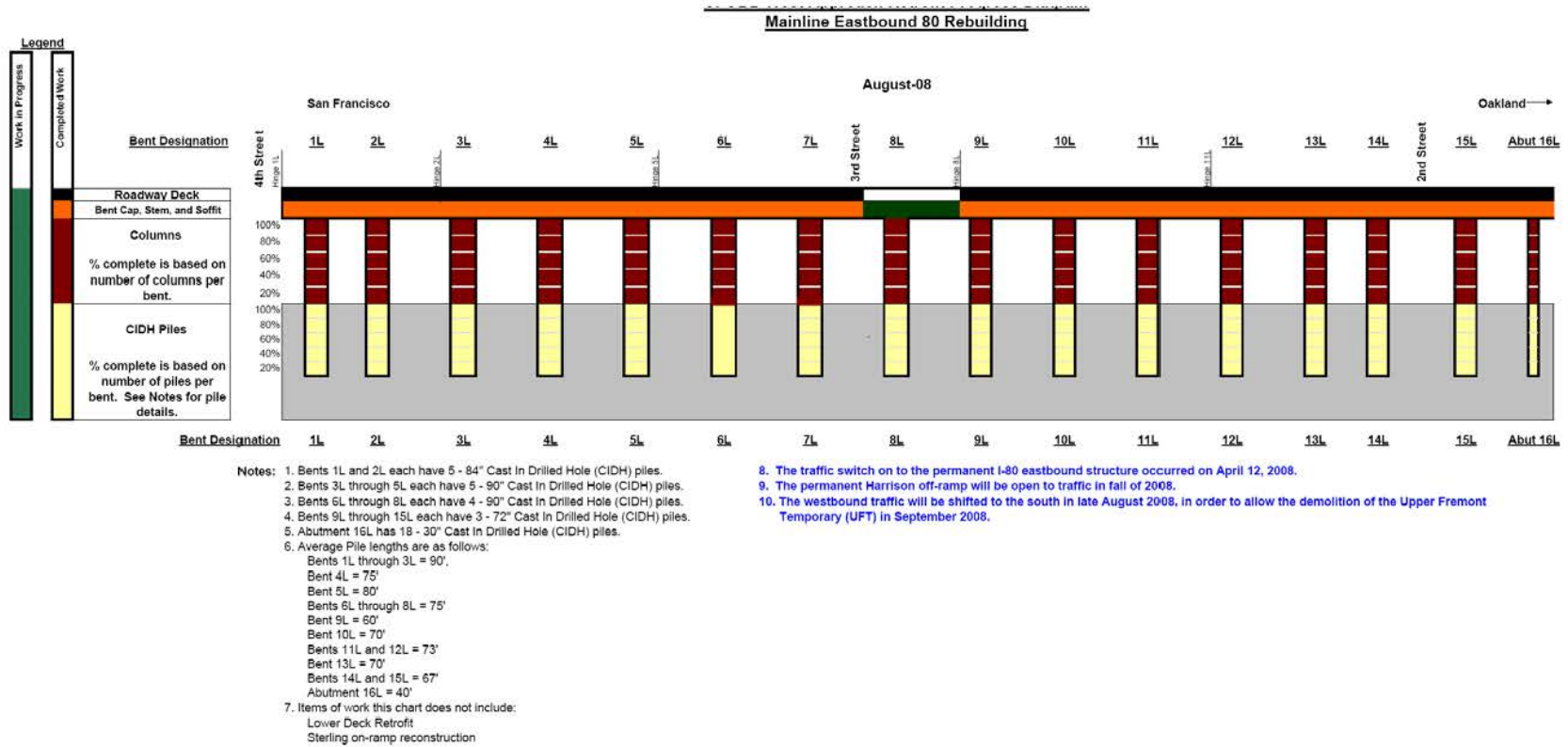
Project Status: Construction is 95% complete as of July 20, 2008, based on expended value of the contract. Seismic retrofit construction is continuing throughout the project. Final widening of both mainline structures is ongoing. Work on the architectural elements of the various retaining walls is on-going. The seismic retrofit work on frame 8L (lower deck anchorage spans) is continuing. The permanent Sterling on-ramp will be open to traffic in September of 2008. The permanent Harrison off-ramp will be open to traffic in the fall of 2008. Westbound traffic will be shifted to the south late August 2008, in order to allow the demolition of the Upper Fremont Temporary (UFT) in September 2008. Job walks with the City of San Francisco (DPT & DPW) to close out items that involve City concurrence is on going, and change orders are being developed to address these issues. Punch list activities for the Fremont off-ramp is ongoing.

Project Issues: None.

Contract Issues: None.

Recent TBPOC Actions: CCO # 13, Supplement. # 11 was approved by TBPOC during their July 10, 2008 meeting.

Project Issues: None.



Contract Photographs



Contract Photographs (cont.)





Toll Bridge Seismic Retrofit Program

Richmond-San Rafael Bridge (RSRB) Seismic Retrofit Project

Project Description: The Richmond-San Rafael (RSR) Bridge Seismic Retrofit Project strengthened the existing bridge to withstand the effects of a large seismic event. As part of the retrofit work, Caltrans performed work to strengthen the bridge foundations, replace the existing west trestle and the main channel fenders and complete the joint rehabilitation of the bridge deck. (The RM1 work is reported in the RM1 section of the report.)

RSRB Seismic Retrofit Cost Summary (\$ Millions)

| Project | AB 144 / SB 66 Budget (07/2005) | Approved Changes | Current Approved Budget (07/2008) | Cost To Date (06/2008) | Cost Forecast (07/2008) | Variance |
|-----------------------------------------------|---------------------------------------|---------------------|--------------------------------------------|------------------------------|-------------------------------|-----------|
| a | b | c | d = b + c | e | f | g = f - d |
| RSRB Seismic Retrofit | | | | | | |
| Capital Outlay Support | 134.0 | (7.0) | 127.0 | 126.7 | 127.0 | - |
| Capital Outlay Construction & Right-of-Way | 780.0 | (90.5) | 689.5 | 666.6 | 689.5 | - |
| TOTAL | 914.0 | (97.5) | 816.5 | 793.3 | 816.5 | - |

Note: Details may not sum to totals due to rounding effects.

* The seismic retrofit contract included work to rehabilitate the bridge deck joints. Although the deck joint work was funded from RM1 toll funds, the work is also eligible for Toll Bridge Seismic Retrofit Program funding. In July 2005, BATA rescinded \$16.9 million in RM1 funds for the deck joint work to make additional RM1 funds available for the New Benicia-Martinez Bridge Project. An equivalent amount of seismic funds will be used on the deck joint work, which is included in the budget above.

RSRB Seismic Retrofit Schedule Summary

| Project | AB 144/SB 66 Project Completion Baseline (07/2005) | Approved Changes (Months) | Project Complete Current Approved Schedule (07/2008) | Contract Complete Schedule Forecast (07/2008) | Schedule Variance (Months) |
|---------------------------|----------------------------------------------------------------|---------------------------------|---------------------------------------------------------------|-----------------------------------------------------------|----------------------------------|
| RSRB Seismic Retrofit | August 2005 | - | August 2005 | October 2005 | 2 |
| RSRB Public Access Lot | NA | - | September 2007 | August 2007 | -1 |

Project Status: The retrofit construction contract was completed and accepted on October 28, 2005. Project savings in the amount of \$97.5 million was transferred to the program contingency.

Construction work on the Public Access Project was completed in August 2007 and the lot was opened to public use.

Recent TBPOC Actions: None.

Toll Bridge Seismic Retrofit Program

Other Completed Seismic Retrofit Projects

Summary Description: Caltrans has already completed the seismic retrofits of the West Spans of the SFOBB, the existing 1958 Carquinez Bridge, the existing Benicia-Martinez Bridge, the San Mateo-Hayward Bridge, and two former toll bridges in Southern California.

Other Completed Seismic Retrofit Projects Cost Summary (\$ Millions)

| Project | AB 144 / SB 66 Budget (07/2005) | Approved Changes | Current Approved Budget (07/2008) | Cost To Date (06/2008) | Cost Forecast (07/2008) | Variance |
|------------------------------------------------------------------------|------------------------------------------|---------------------|--------------------------------------------|------------------------------|-------------------------------|-----------|
| a | b | c | d = b + c | e | f | g = f - d |
| San Francisco-Oakland Bay Bridge West Span Seismic Retrofit Project | 307.9 | - | 307.9 | 301.1 | 307.9 | - |
| Carquinez Bridge Retrofit Project | 114.2 | - | 114.2 | 114.2 | 114.2 | - |
| Benicia-Martinez Bridge Retrofit Project | 177.8 | - | 177.8 | 177.8 | 177.8 | - |
| San Mateo-Hayward Bridge Retrofit Project | 163.5 | - | 163.5 | 163.4 | 163.5 | - |
| Vincent Thomas Bridge Retrofit Project | 58.5 | - | 58.5 | 58.4 | 58.5 | - |
| San Diego-Coronado Bridge Retrofit Project | 103.5 | - | 103.5 | 102.6 | 103.5 | - |
| TOTAL | 925.4 | - | 925.4 | 917.5 | 925.4 | - |

Note: Details may not sum to totals due to rounding effects. Capital Outlay Support and Capital Outlay have been combined.

Other Completed Seismic Retrofit Projects Schedule Summary

| Project | Actual Project Completion Date |
|------------------------------------|--------------------------------|
| Vincent Thomas Bridge Retrofit | May 2000 |
| San Mateo-Hayward Bridge Retrofit | June 2000 |
| Carquinez Bridge Retrofit | January 2003 |
| San Diego-Coronado Bridge Retrofit | June 2003 |
| Benicia-Martinez Bridge Retrofit | August 2003 |
| SFOBB West Span Seismic Retrofit | June 2004 |

Summary Status: Construction has been completed on the above-listed projects. The Estimate at Completion amounts shown above includes allowances for minor project closeout costs.

Contract Issues: None.

Recent TBPOC Actions: None.

Toll Bridge Seismic Retrofit Program

Other Toll Bridges

The Dumbarton Bridge

State Route 84 crosses the southern region of San Francisco Bay between the cities of Newark to the east and East Palo Alto to the west. The Route consists of three lanes in each direction and an eight-foot bicycle/pedestrian lane. The annual average daily traffic (AADT) of the route is near 60,000. The bridge is over 2 km in length and is positioned in an approximately normal geometry between two seismic faults. The United States Geometrical Survey (USGS) reports that the San Andreas Fault, some 15 km to the west of the bridge, and the Hayward Fault, some 13 km to the east of the bridge, pose most of the significant seismic threat to the San Francisco Bay Area.

The Antioch Bridge

State Route 160 crosses the San Joaquin River between the city of Antioch and Sherman Island (leading to Rio Vista) via the Antioch Bridge. The Bridge carries a single lane of traffic in each direction. The AADT for the route is slightly over 10,000 vehicles per day. This bridge is threatened by the Bird's Landing Seismic Zone, Cost Range/Sierra Nevada Boundary Zone and the San Andreas Fault.

Current Progress

Work in the area of bridge structural engineering is continuing for both bridges. The structures team to date has been collecting and evaluating structural information on the bridges and using that information to generate computer models and structural analysis of the bridges. The design team met with other experienced retrofit experts from Caltrans and its consultants in late March to brainstorm the retrofit strategy for the column shear issue. The design team presented their proposed strategy schemes and the results of their analysis to the Peer Review panel. A preliminary cost analysis is also being performed by Caltrans and an independent consultant, Ch2MHill, along with the schedule for design portion of these bridges. A strategy meeting is scheduled for August 22 for both projects and the cost estimate is expected to be completed prior to October 2008.

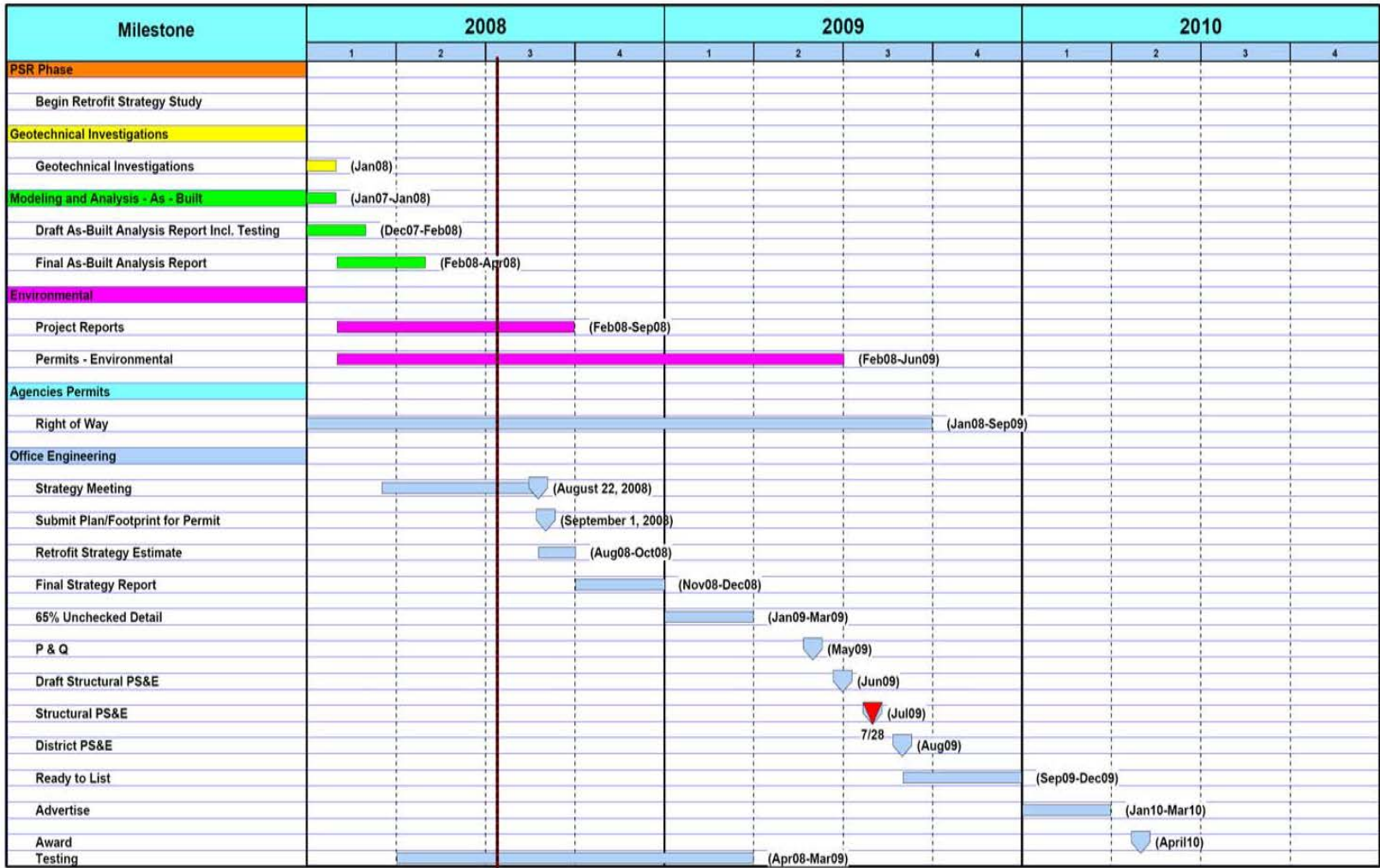
A risk management meeting was held on April 16, 2008 to identify the risks and their impact on cost, scope, and schedule of these projects. The environmental process has begun for both projects and once the design/retrofit strategy is completed, all the permit applications will be submitted to the appropriate agencies for their approval. A meeting was held with the environmental resource agencies on April 22, 2008 to brief them on these projects and to initiate the environmental permit application processes. (See schedule on page 37.)



The Antioch Bridge



The Dumbarton Bridge





PROJECT / CONTRACT REPORTS

Regional Measure 1 Program

New Benicia-Martinez Bridge Project Summary

- New Benicia-Martinez Bridge Contract
- Other Contracts and Related Project Activities

New Carquinez Bridge Project

Richmond-San Rafael Bridge Deck Overlay Project

Interstate 880 / State Route 92 Interchange Reconstruction

Other Completed Regional Measure 1 Projects

- San Mateo–Hayward Bridge Widening Project
- Richmond Parkway Project
- Bayfront Expressway Widening Project
- Richmond-San Rafael Bridge Trestle, Fender, and Deck Joint Rehabilitation Project

Regional Measure 1 Program

New Benicia-Martinez Bridge Project Summary

Project Description: The new Benicia-Martinez Bridge project has constructed a new parallel bridge just east of the existing bridge. The project includes reconstructed interchanges to the north and south of the bridges and a new toll plaza and administration building in Martinez.

New Benicia-Martinez Bridge Project Cost Summary (\$ Millions)

| Contract | BATA Budget (07/2005) | Approved Changes | Current Approved Budget (07/2008) | Cost To Date (07/2008) | Cost Forecast (07/2008) | Variance |
|-----------------------------------------------|-----------------------|------------------|-----------------------------------|------------------------|-------------------------|-----------|
| a | b | c | d = b + c | e | f | g = f - d |
| Capital Outlay Support | 157.1 | 35.2 | 192.3 | 182.6 | 192.3 | - |
| Right-of-Way and Others | 20.4 | (0.1) | 20.3 | 16.9 | 20.3 | - |
| Capital Outlay | | | | | | - |
| New Bridge | 672.0 | 94.6 | 766.6 | 763.8 | 766.6 | - |
| I-680/I-780 Interchange Replacement | 76.3 | 26.9 | 103.2 | 98.5 | 103.2 | - |
| I-680/Marina Vista Interchange Reconstruction | 51.5 | 4.9 | 56.4 | 56.1 | 56.4 | - |
| New Toll Plaza | 24.3 | 2.0 | 26.3 | 23.3 | 26.3 | - |
| Existing Bridge & Interchange Modifications | 17.2 | 42.3 | 59.5 | 10.1 | 59.5 | - |
| Other | 20.3 | 2.8 | 23.1 | 15.4 | 23.1 | - |
| Project Reserve | 20.8 | 4.0 | 24.8 | - | 24.8 | - |
| TOTAL | 1,059.9 | 212.6 | 1,272.5 | 1,166.7 | 1,272.5 | - |

Note: Details may not sum to totals due to rounding effects.

* The budget and estimate at completion includes approximately \$33 million in non-toll bridge funds (Proposition 192 and SHOPP).

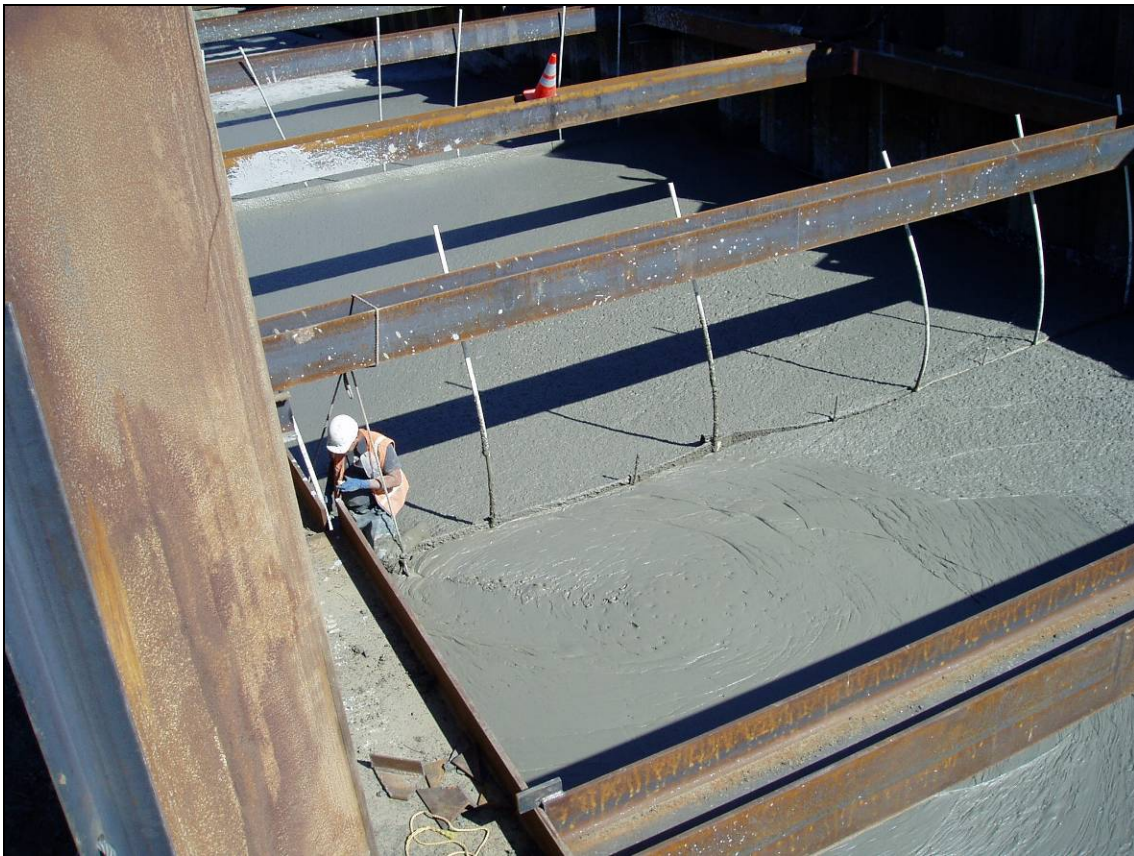
New Benicia-Martinez Bridge Project Schedule Summary

| Contract | BATA Contract Completion Baseline (07/2005) | Approved Changes (Months) | Contract Complete Current Approved Schedule (07/2008) | Contract Complete Schedule Forecast (07/2008) | Schedule Variance (Months) |
|-----------------------------------------------|---------------------------------------------|---------------------------|-------------------------------------------------------|-----------------------------------------------|----------------------------|
| I-680/Marina Vista Interchange Reconstruction | March 2006 | 1 | April 2006 | April 2006 | - |
| New Toll Plaza | June 2006 | - | May 2007 | May 2007 | - |
| New Benicia-Martinez Bridge | December 2007 | - | October 2007 | October 2007 | - |
| I-680/I-780 Interchange Replacement | December 2007 | - | December 2007 | March 2008 | 3 |
| Open to Traffic | December 2007 | - | August 2007 | August 2007 | - |
| Existing Bridge & Interchange Modifications | December 2009 | - | December 2009 | December 2009 | - |

Contract Status:**Existing Bridge & Interchange Modifications:**

The Modification Contract was awarded to American Civil Constructors and Top Grade Construction Joint Venture on November 21, 2007. The 1st contract work day was on January 14, 2008. The contract is expected to take approximately two years. The Contractors continue to submit RFIs and submittals, which are being processed by Caltrans, on a continuous basis. As of July 30, 2008, the project is 30% complete based on schedule. For all the Stage 1 construction work, all joint and full deck replacement work has been completed on both the existing Benicia-Martinez and Mococo bridges. The SB/ES lines have been paved, drainage inlets adjusted, grading work contoured on SB line, earthwork and retaining wall construction at the north side of the bridge. At the south side of the bridge, roadway excavation was completed (class II) at the undulation area behind the sheet piles, and the placement of the lightweight cellular embankment material and installation of the drainage system was completed. The start of the stage two construction is the shifting of the southbound traffic to the newly completed sections of the Benicia-Martinez and Mococco bridges and the interconnecting roadways on both ends of the bridges, is scheduled for the weekend of August the 15th.

Recent TBPOC Actions: None.



Cellular Concrete at Undulation Area 5

Contract Photographs

Looking South at the New SB680



View of the SB 680 Looking North from the North End of the Bridge



Looking North at the New SB680



View of the SB 680 Looking South from the North End of the Bridge

Regional Measure 1 Program

New Carquinez Bridge Project

Project Description: The New Carquinez Bridge project involves constructing a new suspension bridge west of the existing bridges with four westbound lanes and a bicycle/pedestrian lane and demolishing the existing 1927 bridge.

New Carquinez Bridge Cost Summary (\$ Millions)

| Contract | BATA Budget (07/2005) | Approved Changes | Current Approved Budget (07/2008) | Cost To Date (07/2008) | Cost Forecast (07/2008) | Variance |
|-----------------------------|-----------------------|------------------|-----------------------------------|------------------------|-------------------------|--------------|
| a | b | c | d = b + c | e | f | g = f - d |
| Capital Outlay Support | 124.4 | (0.2) | 124.2 | 123.5 | 123.6 | (0.6) |
| Capital Outlay Construction | | | | | | - |
| Replacement Bridge | 253.3 | 4.0 | 257.3 | 255.3 | 257.3 | - |
| South Interchange | 73.9 | - | 73.9 | 71.9 | 73.9 | - |
| Existing 1927 Bridge | 35.2 | - | 35.2 | 34.8 | 35.2 | - |
| Other | 29.3 | (0.8) | 28.5 | 24.0 | 28.6 | 0.1 |
| Project Reserve | 12.1 | (3.0) | 9.1 | - | 0.6 | (8.5) |
| TOTAL | 528.2 | - | 528.2 | 509.5 | 519.2 | (9.0) |

Note: Details may not sum to totals due to rounding effects.

New Carquinez Bridge Schedule Summary

| Contract | BATA Contract Completion Baseline (07/2005) | Approved Changes (Months) | Contract Complete Current Approved Schedule (07/2008) | Contract Complete Schedule Forecast (07/2008) | Schedule Variance (Months) |
|----------------------------------|---------------------------------------------|---------------------------|-------------------------------------------------------|-----------------------------------------------|----------------------------|
| New Carquinez Bridge | December 2003* | - | December 2003* | December 2003* | - |
| 1927 Carquinez Bridge Demolition | September 2007 | - | December 2007 | December 2007 | 3 |
| Landscaping | August 2011 | - | August 2011 | August 2011 | - |

* The date shown is for the opening of the bridge to traffic.

Project Status: The new replacement bridge and all its approaches have been completed and were opened to traffic in November 2003. The removal of the entire 1927 bridge (Main Truss) was completed in September 2007. The Carquinez Bridge Demolition Contract was substantially complete in December 2007 and accepted in June 2008. The contract was signed on June 3, 2008.

Project Issues: None

Regional Measure 1 Program

Interstate 880/State Route 92 Interchange Reconstruction Project

Project Description: Modify the existing cloverleaf interchange to increase capacity and improve safety and traffic operations.

Interstate 880/State Route 92 Interchange Cost Summary (\$ Millions)

| Contract | BATA Budget (07/2005) | Approved Changes | Current Approved Budget (07/2008) | Cost To Date (07/2008) | Cost Forecast (07/2008) | Variance |
|-------------------------------------|-----------------------|------------------|-----------------------------------|------------------------|-------------------------|-----------|
| a | b | c | d = b + c | e | f | g = f - d |
| I-880/SR-92 Interchange Improvement | | | | | | |
| Capital Outlay Support | 28.8 | 26.2 | 55.0 | 40.8 | 55.0 | - |
| Capital Outlay Construction | 94.8 | 60.2 | 155.0 | 29.6 | 155.0 | - |
| Capital Outlay Right-of-Way | 9.9 | 5.1 | 16.9 | 11.0 | 16.9 | - |
| Project Reserve | 0.3 | 19.7 | 18.1 | - | 18.1 | - |
| TOTAL | 133.8 | 111.2 | 245.0 | 81.4 | 245.0 | - |

Note: Details may not sum to totals due to rounding effects. \$9.6 million in ACTA funds included under Capital Outlay Construction. \$3.0 million included in Capital Outlay Construction and \$1.0 million in Capital Outlay Support for separate landscape contract.

Interstate 880/State Route 92 Interchange Schedule Summary

| Project | BATA Project Completion Baseline (07/2005) | Approved Changes (Months) | Project Complete Current Approved Schedule (07/2008) | Contract Complete Schedule Forecast (07/2008) | Schedule Variance (Months) |
|----------------------------------------|--------------------------------------------|---------------------------|------------------------------------------------------|-----------------------------------------------|----------------------------|
| I-880/SR-92 Interchange Reconstruction | December 2010 | - | June 2011 | June 2011 | 6 |

Project Status: On August 28, 2007, Caltrans awarded the Interstate 880/State Route 92 Interchange Reconstruction contract to the joint venture of FCI and Granite Construction for \$138.4 million.

The construction contract was approved on September 28, 2007. The 1st contract day of the project was October 26, 2007.

The contract schedule is 27% complete as of the end of July 2008, based on expended value of the contract. The contractor has completed 55% of the EB SR-92 to NB I-880 connector bridge. The 45-day settlement period was completed for the EB SR-92 to NB I-880 for abutment one. Falsework at EB SR-92 to NB I-880 connector bridge is currently 31% complete. Work at retaining walls for EB SR-92 continues and the retaining wall "A" section one is complete and the RW "A" section two to begin by mid August. The remainder of retaining wall "G" with the soundwall is scheduled to be completed by mid November 2008. This retaining wall work will allow for the widening of the SR-92 portion of the project, and will eventually allow access to the major bridge work that remains. Work is ongoing to complete the temporary Calaroga Avenue overcrossing of SR-92 and the pedestrian overcrossing at Eldridge Avenue over I-880. Retaining wall "F1" is now 20% complete.

Contract Photographs

Abutment 8 under Construction with Bent 7, & 6 Columns to the Right



Falsework Continues at West Side of EB SR92 to NB I-880 Connector Bridge (ENCONN). Bent 5 Columns are to the Left at I-880 Median

Contract Photographs (cont.)

Interstate 880/State Route 92 Interchange Reconstruction Project



Interstate 880/State Route 92 Interchange Reconstruction Project

Project Photographs

Interstate 880/State Route 92
Interchange
BEFORE



Interstate 880/State Route 92
Interchange
AUGUST 2008



Interstate 880/State Route 92
Interchange
AFTER

Regional Measure 1 Program

Other Completed Regional Measure 1 (RM1) Projects

Summary Description: Other completed Regional Measure 1 projects are the following: (a) Widen the San Mateo-Hayward Bridge along its low-trestle section and its eastern approach; (b) Widen the Bayfront Expressway (SR 84) from the Dumbarton Bridge to the U.S. 101/Marsh Road interchange; (c) Construct an eastern approach (Richmond Parkway) between the Richmond-San Rafael Bridge and Interstate 80 near Pinole; (d) Modify the U.S. 101/University Avenue interchange; (e) Richmond-San Rafael Bridge Trestle, Fender and Deck Joint Rehabilitation Project; and (f) Richmond-San Rafael Bridge Deck Overlay Project.

Other Completed RM1 Projects Cost Summary (\$ Millions)

| Contract | BATA Budget (07/2005) | Approved Changes | Current Approved Budget (07/2008) | Cost To Date (07/2008) | Cost Forecast (07/2008) | Variance |
|-----------------------------------------------|-----------------------------|---------------------|--------------------------------------------|------------------------------|-------------------------------|---------------|
| a | b | c | d = b + c | e | f | g = f - d |
| San Mateo-Hayward Bridge Widening Project | 217.8 | - | 217.8 | 208.7 | 211.9 | (5.9) |
| Bayfront Expressway Widening Project | 36.1 | - | 36.1 | 33.4 | 36.0 | (0.1) |
| Richmond Parkway Project | 5.9 | - | 5.9 | 4.3 | 5.9 | - |
| U.S. 101/University Interchange | 3.8 | - | 3.8 | 3.7 | 3.8 | - |
| RSR Trestle, Fender, and Joint Rehabilitation | 103.1 | - | 103.1 | 96.3 | 97.1 | (5.0) |
| RSR Deck Overlay | 25.0 | - | 25.0 | 19.6 | 25.0 | - |
| TOTAL | 390.7 | - | 390.7 | 366.0 | 379.7 | (11.0) |

Schedule Summary

| Project | Actual Project Completion Date |
|--------------------------------------------------------------------------|--------------------------------|
| Richmond Parkway Project | May 2001 |
| San Mateo-Hayward Bridge Widening Project | February 2003 |
| Bayfront Expressway Widening Project | January 2004 |
| U.S. 101/University Interchange | April 2004 |
| Richmond-San Rafael Bridge Trestle, Fender and Deck Joint Rehabilitation | August 2005 |
| RSR Deck Overlay | December 2006 |

Project Status: Construction has been completed on the above listed contracts.

Project Issues: None.



APPENDICES

- A** Toll Bridge Seismic Retrofit Program:
San Francisco-Oakland Bay Bridge (SFOBB) East Span Replacement Project Cost
Detail
- B** Toll Bridge Seismic Retrofit Program Cost Detail
- C** Toll Bridge Seismic Retrofit Program Summary Schedule
- D** Regional Measure 1 Program Cost Detail
- E** Regional Measure 1 Program Summary Schedule

** Forecasts for the Monthly Reports are generally updated on a quarterly basis in conjunction with Risk Analysis assessments for the TBSRP Projects and the TBSRP Quarterly Reports.*

Appendix A: Toll Bridge Seismic Retrofit Program (\$ Millions)

San Francisco-Oakland Bay Bridge (SFOBB) East Span Replacement Project Cost Detail

| Contract | EA Number | AB 144 / SB 66 Budget (07/2005) | Approved Changes | Current Approved Budget (06/2008) | Cost To Date (06/2008) | Cost Forecast (06/2008) | At-Completion Variance |
|-----------------------------------------------------------------------|---------------|---------------------------------------|---------------------|-----------------------------------------|---------------------------|-------------------------------|---------------------------|
| a | b | c | d | e = c + d | f | g | h = g - e |
| San Francisco-Oakland Bay Bridge East Span Replacement Project | | | | | | | |
| East Span - Skyway | 01202X | | | | | | |
| Capital Outlay Support | | 197.0 | (16.0) | 181.0 | 180.1 | 181.0 | - |
| Capital Outlay Construction | | 1,293.0 | (38.9) | 1,254.1 | 1,234.1 | 1,254.1 | - |
| Total | | 1,490.0 | (54.9) | 1,435.1 | 1,414.2 | 1,435.1 | - |
| East Span - SAS E2/T1 Foundations | 0120EX | | | | | | |
| Capital Outlay Support | | 52.5 | (21.5) | 31.0 | 28.1 | 31.0 | - |
| Capital Outlay Construction | | 313.5 | (32.6) | 280.9 | 272.7 | 280.9 | - |
| Total | | 366.0 | (54.1) | 311.9 | 300.8 | 311.9 | - |
| East Span - SAS Superstructure | 0120FX | | | | | | |
| Capital Outlay Support | | 214.6 | - | 214.6 | 91.9 | 214.6 | - |
| Capital Outlay Construction | | 1,753.7 | - | 1,753.7 | 466.0 | 1,767.4 | 13.7 |
| Total | | 1,968.3 | - | 1,968.3 | 557.9 | 1,982.0 | 13.7 |
| SAS W2 Foundations | 0120CX | | | | | | |
| Capital Outlay Support | | 10.0 | - | 10.0 | 9.2 | 10.0 | - |
| Capital Outlay Construction | | 26.4 | - | 26.4 | 25.8 | 26.4 | - |
| Total | | 36.4 | - | 36.4 | 35.0 | 36.4 | - |
| YBI South/South Detour | 0120RX | | | | | | |
| Capital Outlay Support | | 29.4 | 36.6 | 66.0 | 44.7 | 66.0 | - |
| Capital Outlay Construction | | 132.0 | 310.2 | 442.2 | 191.4 | 461.2 | 19.0 |
| Total | | 161.4 | 346.8 | 508.2 | 236.1 | 527.2 | 19.0 |
| YBI Transition Structures (see notes below) | 0120PX | | | | | | |
| Capital Outlay Support | | 78.7 | - | 78.7 | 20.3 | 78.7 | - |
| Capital Outlay Construction | | 299.3 | (23.2) | 276.1 | - | 276.1 | - |
| Total | | 378.0 | (23.2) | 354.8 | 20.3 | 354.8 | - |
| * YBI- Transition Structures Contract No. 1 | | | | | | | |
| Capital Outlay Support | | | | | 2.7 | 45.0 | |
| Capital Outlay Construction | | | | | - | 214.3 | |
| Total | | | | | 2.7 | 259.3 | |
| * YBI- Transition Structures Contract No. 2 | | | | | | | |
| Capital Outlay Support | | | | | 1.2 | 16.0 | |
| Capital Outlay Construction | | | | | - | 58.5 | |
| Total | | | | | 1.2 | 74.5 | |
| * YBI- Transition Structures Contract No. 3 Landscape | | | | | | | |
| Capital Outlay Support | | | | | - | 1.0 | |
| Capital Outlay Construction | | | | | - | 3.3 | |
| Total | | | | | - | 4.3 | |
| Oakland Touchdown (see notes below) | 01204X | | | | | | |
| Capital Outlay Support | | 74.4 | - | 74.4 | 38.0 | 92.1 | 17.7 |
| Capital Outlay Construction | | 283.8 | - | 283.8 | 102.4 | 302.5 | 18.7 |
| Total | | 358.2 | - | 358.2 | 140.4 | 394.6 | 36.4 |
| * OTD Submarine Cable | 0120K4 | | | | | | |
| Capital Outlay Support | | | | | 0.9 | 3.0 | |
| Capital Outlay Construction | | | | | 7.9 | 9.6 | |
| Total | | | | | 8.8 | 12.6 | |
| * OTD No. 1 (Westbound) | 0120L4 | | | | | | |
| Capital Outlay Support | | | | | 16.4 | 49.9 | |
| Capital Outlay Construction | | | | | 94.5 | 226.5 | |
| Total | | | | | 110.9 | 276.4 | |
| * OTD No. 2 (Eastbound) | 0120M4 | | | | | | |
| Capital Outlay Support | | | | | 0.6 | 15.8 | |
| Capital Outlay Construction | | | | | - | 62.0 | |
| Total | | | | | 0.6 | 77.8 | |
| * OTD Electrical Systems | 0120N4 | | | | | | |
| Capital Outlay Support | | | | | 0.1 | 1.4 | |
| Capital Outlay Construction | | | | | - | 4.4 | |
| Total | | | | | 0.1 | 5.8 | |

Notes: YBI Transition Structures and Oakland Touchdown Cost-to-Date and Cost Forecast includes prior-to-split Capital Outlay Support Costs.

Note: Details may not sum to totals due to rounding effects.

Appendix A: Toll Bridge Seismic Retrofit Program (\$ Millions)

San Francisco-Oakland Bay Bridge (SFOBB) East Span Replacement Project Cost Detail (Cont'd.)

| Contract | EA Number | AB 144 / SB 66 Budget (07/2005) | Approved Changes | Current Approved Budget (06/2008) | Cost To Date (06/2008) | Cost Forecast (06/2008) | At-Completion Variance |
|--------------------------------------------------|----------------------------|---------------------------------------|---------------------|-----------------------------------------|---------------------------|-------------------------------|---------------------------|
| a | b | c | d | e = c + d | f | g | h = g - e |
| Existing Bridge Demolition | 01209X | | | | | | |
| Capital Outlay Support | | 79.7 | - | 79.7 | 0.3 | 79.7 | - |
| Capital Outlay Construction | | 239.2 | - | 239.2 | - | 222.0 | (17.2) |
| Total | | 318.9 | - | 318.9 | 0.3 | 301.7 | (17.2) |
| YBI/SAS Archeology | 01207X | | | | | | |
| Capital Outlay Support | | 1.1 | - | 1.1 | 1.1 | 1.1 | - |
| Capital Outlay Construction | | 1.1 | - | 1.1 | 1.1 | 1.1 | - |
| Total | | 2.2 | - | 2.2 | 2.2 | 2.2 | - |
| YBI - USCG Road Relocation | 0120QX | | | | | | |
| Capital Outlay Support | | 3.0 | - | 3.0 | 2.7 | 3.0 | - |
| Capital Outlay Construction | | 3.0 | - | 3.0 | 2.8 | 3.0 | - |
| Total | | 6.0 | - | 6.0 | 5.5 | 6.0 | - |
| YBI - Substation and Viaduct | 0120GX | | | | | | |
| Capital Outlay Support | | 6.5 | - | 6.5 | 6.4 | 6.5 | - |
| Capital Outlay Construction | | 11.6 | - | 11.6 | 11.3 | 11.6 | - |
| Total | | 18.1 | - | 18.1 | 17.7 | 18.1 | - |
| Oakland Geofill | 01205X | | | | | | |
| Capital Outlay Support | | 2.5 | - | 2.5 | 2.5 | 2.5 | - |
| Capital Outlay Construction | | 8.2 | - | 8.2 | 8.2 | 8.2 | - |
| Total | | 10.7 | - | 10.7 | 10.7 | 10.7 | - |
| Pile Installation Demonstration Project | 01208X | | | | | | |
| Capital Outlay Support | | 1.8 | - | 1.8 | 1.8 | 1.8 | - |
| Capital Outlay Construction | | 9.2 | - | 9.2 | 9.2 | 9.2 | - |
| Total | | 11.0 | - | 11.0 | 11.0 | 11.0 | - |
| Stormwater Treatment Measures | 0120JX | | | | | | |
| Capital Outlay Support | | 6.0 | 2.0 | 8.0 | 7.9 | 8.0 | - |
| Capital Outlay Construction | | 15.0 | 3.3 | 18.3 | 16.4 | 18.3 | - |
| Total | | 21.0 | 5.3 | 26.3 | 24.3 | 26.3 | - |
| Right-of-Way and Environmental Mitigation | 0120X9 | | | | | | |
| Capital Outlay Support | | - | - | - | - | - | - |
| Capital Outlay & Right-of-Way | | 72.4 | - | 72.4 | 39.3 | 72.4 | - |
| Total | | 72.4 | - | 72.4 | 39.3 | 72.4 | - |
| | 04343X & 04300X | | | | | | |
| Sunk Cost - Existing East Span Retrofit | | | | | | | |
| Capital Outlay Support | | 39.5 | - | 39.5 | 39.5 | 39.5 | - |
| Capital Outlay Construction | | 30.8 | - | 30.8 | 30.8 | 30.8 | - |
| Total | | 70.3 | - | 70.3 | 70.3 | 70.3 | - |
| Other Capital Outlay Support | | | | | | | |
| Environmental Phase | | 97.7 | - | 97.7 | 97.7 | 97.7 | - |
| Pre-Split Project Expenditures | | 44.9 | - | 44.9 | 44.9 | 44.9 | - |
| Non-project Specific Costs | | 20.0 | (1.0) | 19.0 | 3.2 | 19.0 | - |
| Total | | 162.6 | (1.0) | 161.6 | 145.8 | 161.6 | - |
| Subtotal Capital Outlay Support | | 959.3 | - | 959.3 | 620.3 | 977.1 | 17.7 |
| Subtotal Capital Outlay Construction | | 4,492.2 | 218.8 | 4,711.0 | 2,411.5 | 4,745.2 | 34.2 |
| Other Budgeted Capital | | 35.1 | (3.3) | 31.8 | 0.7 | 7.7 | (24.1) |
| Total SFOBB East Span Replacement Project | | 5,486.6 | 215.5 | 5,702.1 | 3,032.5 | 5,730.0 | 27.9 |

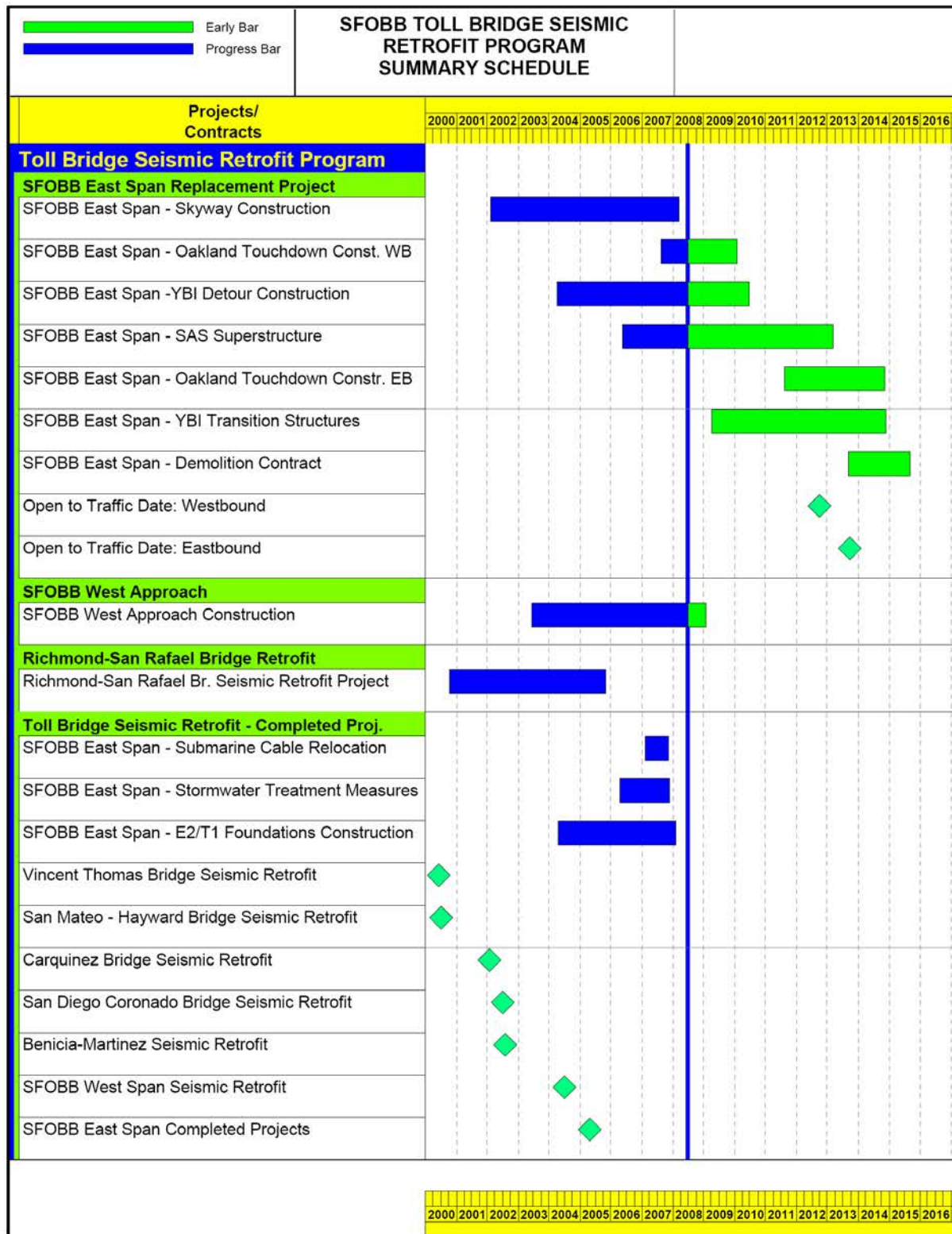
Note: Details may not sum to totals due to rounding effects.

Appendix B: Toll Bridge Seismic Retrofit Program Cost Detail (\$ Millions)

| Contract | AB 144 / SB 66 Budget (07/2005) | Approved Changes | Current Approved Budget (06/2008) | Cost To Date (06/2008) | Cost Forecast (06/2008) | At-Completion Variance |
|------------------------------------------------------|---------------------------------------|---------------------|-----------------------------------------|---------------------------|-------------------------------|---------------------------|
| a | c | d | e = c + d | f | g | h = g - e |
| SFOBB East Span Replacement Project | | | | | | |
| Capital Outlay Support | 959.3 | - | 959.3 | 620.3 | 977.1 | 17.8 |
| Capital Outlay Construction | 4,492.2 | 218.8 | 4,711.0 | 2,411.5 | 4,745.2 | 34.2 |
| Other Budgeted Capital | 35.1 | (3.3) | 31.8 | 0.7 | 7.7 | (24.1) |
| Total | 5,486.6 | 215.5 | 5,702.1 | 3,032.5 | 5,730.0 | 27.9 |
| SFOBB West Approach Replacement | | | | | | |
| Capital Outlay Support | 120.0 | - | 120.0 | 107.4 | 120.0 | - |
| Capital Outlay Construction | 309.0 | 24.7 | 333.7 | 282.9 | 350.7 | 17.0 |
| Total | 429.0 | 24.7 | 453.7 | 390.3 | 470.7 | 17.0 |
| SFOBB West Span Retrofit | | | | | | |
| Capital Outlay Support | 75.0 | - | 75.0 | 74.8 | 75.0 | - |
| Capital Outlay Construction | 232.9 | - | 232.9 | 226.3 | 232.9 | - |
| Total | 307.9 | - | 307.9 | 301.1 | 307.9 | - |
| Richmond-San Rafael Bridge Retrofit | | | | | | |
| Capital Outlay Support | 134.0 | (7.0) | 127.0 | 126.7 | 127.0 | - |
| Capital Outlay Construction | 780.0 | (90.5) | 689.5 | 666.6 | 689.5 | - |
| Total | 914.0 | (97.5) | 816.5 | 793.3 | 816.5 | - |
| Benicia-Martinez Bridge Retrofit | | | | | | |
| Capital Outlay Support | 38.1 | - | 38.1 | 38.1 | 38.1 | - |
| Capital Outlay Construction | 139.7 | - | 139.7 | 139.7 | 139.7 | - |
| Total | 177.8 | - | 177.8 | 177.8 | 177.8 | - |
| Carquinez Bridge Retrofit | | | | | | |
| Capital Outlay Support | 28.7 | - | 28.7 | 28.8 | 28.7 | - |
| Capital Outlay Construction | 85.5 | - | 85.5 | 85.4 | 85.5 | - |
| Total | 114.2 | - | 114.2 | 114.2 | 114.2 | - |
| San Mateo-Hayward Bridge Retrofit | | | | | | |
| Capital Outlay Support | 28.1 | - | 28.1 | 28.1 | 28.1 | - |
| Capital Outlay Construction | 135.4 | - | 135.4 | 135.3 | 135.4 | - |
| Total | 163.5 | - | 163.5 | 163.4 | 163.5 | - |
| Vincent Thomas Bridge Retrofit (Los Angeles) | | | | | | |
| Capital Outlay Support | 16.4 | - | 16.4 | 16.4 | 16.4 | - |
| Capital Outlay Construction | 42.1 | - | 42.1 | 42.0 | 42.1 | - |
| Total | 58.5 | - | 58.5 | 58.4 | 58.5 | - |
| San Diego-Coronado Bridge Retrofit | | | | | | |
| Capital Outlay Support | 33.5 | - | 33.5 | 33.2 | 33.5 | - |
| Capital Outlay Construction | 70.0 | - | 70.0 | 69.4 | 70.0 | - |
| Total | 103.5 | - | 103.5 | 102.6 | 103.5 | - |
| Subtotal Capital Outlay Support | 1,433.1 | (7.0) | 1,426.1 | 1,073.8 | 1,443.9 | 17.8 |
| Subtotal Capital Outlay | 6,286.8 | 153.0 | 6,439.8 | 4,059.1 | 6,491.0 | 51.2 |
| Subtotal Other Budgeted Capital | 35.1 | (3.3) | 31.8 | 0.7 | 7.7 | (24.1) |
| Miscellaneous Program Costs | 30.0 | - | 30.0 | 24.7 | 30.0 | - |
| Subtotal Toll Bridge Seismic Retrofit Program | 7,785.0 | 142.7 | 7,927.7 | 5,158.3 | 7,972.6 | 44.9 |
| Program Contingency | 900.0 | (142.7) | 757.3 | - | 712.4 | (44.9) |
| Total Toll Bridge Seismic Retrofit Program | 8,685.0 | - | 8,685.0 | 5,158.3 | 8,685.0 | - |

Note: Details may not sum to totals due to rounding effects.

Appendix C: Toll Bridge Seismic Retrofit Program Summary Schedule



Appendix D: Regional Measure 1 Program Cost Detail (\$ Millions)

| Project | EA Number | BATA Budget (07/2005) | Approved Changes | Current Approved Budget (07/2008) | Cost To Date (07/2008) | Cost Forecast (07/2008) | At-Completion Variance |
|--------------------------------------------------------|-----------------------|--------------------------|---------------------|--------------------------------------------|---------------------------|----------------------------|---------------------------|
| a | b | c | d | e = c + d | f | g | h = g - e |
| New Benicia-Martinez Bridge Project | | | | | | | |
| New Bridge | 00603_ | | | | | | |
| Capital Outlay Support | | 84.9 | 6.7 | 91.6 | 91.5 | 91.6 | - |
| Capital Outlay Construction | | | | - | | | - |
| BATA Funding | | 661.9 | 94.6 | 756.5 | 753.7 | 756.5 | - |
| Non-BATA Funding | | 10.1 | - | 10.1 | 10.1 | 10.1 | - |
| Subtotal | | 672.0 | 94.6 | 766.6 | 763.8 | 766.6 | - |
| Total | | 756.9 | 101.3 | 858.2 | 855.3 | 858.2 | - |
| I-680/I-780 Interchange Reconstruction | | | | | | | |
| I-680/I-780 Interchange Reconstruction | 00606_ | | | | | | |
| Capital Outlay Support | | | | | | | |
| BATA Funding | | 24.9 | 5.2 | 30.1 | 29.9 | 30.1 | - |
| Non-BATA Funding | | 1.4 | 5.2 | 6.6 | 6.3 | 6.6 | - |
| Subtotal | | 26.3 | 10.4 | 36.7 | 36.2 | 36.7 | - |
| Capital Outlay Construction | | | | | | | |
| BATA Funding | | 54.7 | 26.9 | 81.6 | 76.8 | 81.6 | - |
| Non-BATA Funding | | 21.6 | - | 21.6 | 21.7 | 21.6 | - |
| Subtotal | | 76.3 | 26.9 | 103.2 | 98.5 | 103.2 | - |
| Total | | 102.6 | 37.3 | 139.9 | 134.7 | 139.9 | - |
| I-680/Marina Vista Interchange Reconstruction | | | | | | | |
| I-680/Marina Vista Interchange Reconstruction | 00605_ | | | | | | |
| Capital Outlay Support | | 18.3 | 1.8 | 20.1 | 19.9 | 20.1 | - |
| Capital Outlay Construction | | 51.5 | 4.9 | 56.4 | 56.1 | 56.4 | - |
| Total | | 69.8 | 6.7 | 76.5 | 76.0 | 76.5 | - |
| New Toll Plaza and Administration Building | | | | | | | |
| New Toll Plaza and Administration Building | 00604_ | | | | | | |
| Capital Outlay Support | | 11.9 | 3.8 | 15.7 | 15.7 | 15.7 | - |
| Capital Outlay Construction | | 24.3 | 2.0 | 26.3 | 23.3 | 26.3 | - |
| Total | | 36.2 | 5.8 | 42.0 | 39.0 | 42.0 | - |
| Existing Bridge & Interchange Modifications | | | | | | | |
| Existing Bridge & Interchange Modifications | 0060A_ | | | | | | |
| Capital Outlay Support | | 4.3 | 14.3 | 18.6 | 12.2 | 18.6 | - |
| Capital Outlay Construction | | | | | | | |
| BATA Funding | | 17.2 | 32.8 | 50.0 | 10.1 | 50.0 | - |
| Non-BATA Funding | | - | 9.5 | 9.5 | - | 9.5 | - |
| Subtotal | | 17.2 | 42.3 | 59.5 | 10.1 | 59.5 | - |
| Total | | 21.5 | 56.6 | 78.1 | 22.3 | 78.1 | - |
| Other Contracts | | | | | | | |
| Other Contracts | See note below | | | | | | |
| Capital Outlay Support | | 11.4 | (1.8) | 9.6 | 7.1 | 9.6 | - |
| Capital Outlay Construction | | 20.3 | 2.8 | 23.1 | 15.4 | 23.1 | - |
| Capital Outlay Right-of-Way | | 20.4 | (0.1) | 20.3 | 16.9 | 20.3 | - |
| Total | | 52.1 | 0.9 | 53.0 | 39.4 | 53.0 | - |
| Subtotal BATA Capital Outlay Support | | 155.7 | 30.0 | 185.7 | 176.3 | 185.7 | - |
| Subtotal BATA Capital Outlay Construction | | 829.9 | 164.0 | 993.9 | 935.4 | 993.9 | - |
| Subtotal Capital Outlay Right-of-Way | | 20.4 | (0.1) | 20.3 | 16.9 | 20.3 | - |
| Subtotal Non-BATA Capital Outlay Support | | 1.4 | 5.2 | 6.6 | 6.3 | 6.6 | - |
| Subtotal Non-BATA Capital Outlay Construction | | 31.7 | 9.5 | 41.2 | 31.8 | 41.2 | - |
| Project Reserves | | 20.8 | 4.0 | 24.8 | - | 24.8 | - |
| Total New Benicia-Martinez Bridge Project | | 1,059.9 | 212.6 | 1,272.5 | 1,166.7 | 1,272.5 | - |

Notes: Includes EA's 00601_, 00603_, 00605_, 00606_, 00608_, 00609_, 0060A_, 0060C_, 0060E_, 0060F_, 0060G_, and 0060H_ and all Project Right-of-Way

Note: Details may not sum to totals due to rounding effects.

Appendix D: Regional Measure 1 Program Cost Detail (\$ Millions) (Cont'd.)

| Project | EA Number | BATA Budget (07/2005) | Approved Changes | Current Approved Budget (06/2008) | Cost To Date (06/2008) | Cost Forecast (06/2008) | At-Completion Variance |
|---------------------------------------------------|-----------------------|--------------------------|---------------------|--------------------------------------------|---------------------------|----------------------------|---------------------------|
| a | b | c | d | e = c + d | f | g | h = g - e |
| Carquinez Bridge Replacement Project | | | | | | | |
| New Bridge | 01301_ | | | | | | |
| Capital Outlay Support | | 60.5 | (0.3) | 60.2 | 60.2 | 60.2 | - |
| Capital Outlay Construction | | 253.3 | 4.0 | 257.3 | 255.4 | 257.3 | - |
| Total | | 313.8 | 3.7 | 317.5 | 315.6 | 317.5 | - |
| Crockett Interchange Reconstruction | 01305_ | | | | | | |
| Capital Outlay Support | | 32.0 | (0.1) | 31.9 | 31.9 | 31.9 | - |
| Capital Outlay Construction | | 73.9 | - | 73.9 | 71.9 | 73.9 | - |
| Total | | 105.9 | (0.1) | 105.8 | 103.8 | 105.8 | - |
| Existing 1927 Bridge Demolition | 01309_ | | | | | | |
| Capital Outlay Support | | 16.1 | - | 16.1 | 15.3 | 15.5 | (0.6) |
| Capital Outlay Construction | | 35.2 | - | 35.2 | 34.7 | 35.2 | - |
| Total | | 51.3 | - | 51.3 | 50.0 | 50.7 | (0.6) |
| Other Contracts | See note below | | | | | | |
| Capital Outlay Support | | 15.8 | 0.2 | 16.0 | 16.0 | 16.0 | - |
| Capital Outlay Construction | | 18.8 | (0.8) | 18.0 | 14.0 | 18.1 | 0.1 |
| Capital Outlay Right-of-Way | | 10.5 | - | 10.5 | 9.9 | 10.5 | - |
| Total | | 45.1 | (0.6) | 44.5 | 39.9 | 44.6 | 0.1 |
| Subtotal BATA Capital Outlay Support | | 124.4 | (0.2) | 124.2 | 123.4 | 123.6 | (0.6) |
| Subtotal BATA Capital Outlay Construction | | 381.2 | 3.2 | 384.4 | 376.0 | 384.5 | 0.1 |
| Subtotal Capital Outlay Right-of-Way | | 10.5 | - | 10.5 | 9.9 | 10.5 | - |
| Project Reserves | | 12.1 | (3.0) | 9.1 | - | 0.6 | (8.5) |
| Total Carquinez Bridge Replacement Project | | 528.2 | - | 528.2 | 509.3 | 519.2 | (9.0) |

Notes:

Other Contracts includes EA's 01301_, 01302_, 01303_, 01304_, 01305_, 01306_, 01307_, 01308_, 01309_, 0130A_, 0130C_, 0130D_, 0130F_, 0130G_, 0130H_, 0130J_, 00453_, 00493_, 04700_, 00607_, 2A270_, and 29920_ and all Project Right-of-Way

Note: Details may not sum to totals due to rounding effects.

Appendix D: Regional Measure 1 Program Cost Detail (\$ Millions) (Cont'd.)

| Project | EA Number | BATA Budget (07/2005) | Approved Changes | Current Approved Budget (07/2008) | Cost To Date (07/2008) | Cost Forecast (07/2008) | At-Completion Variance |
|----------------------------------------------------------------------------------|---------------------------------|--------------------------|---------------------|--------------------------------------------|---------------------------|----------------------------|---------------------------|
| a | b | c | d | e = c + d | f | g | h = g - e |
| Richmond-San Rafael Bridge Trestle, Fender, and Deck Joint Rehabilitation | | | | | | | |
| | See note ¹ below | | | | | | |
| Capital Outlay Support | | | | | | | |
| BATA Funding | | 2.2 | - | 2.2 | 1.4 | 2.2 | - |
| Non-BATA Funding | | 8.6 | - | 8.6 | 10.4 | 10.4 | 1.8 |
| Subtotal | | 10.8 | - | 10.8 | 11.8 | 12.6 | 1.8 |
| Capital Outlay Construction | | | | | | | |
| BATA Funding | | 40.2 | - | 40.2 | 33.4 | 33.4 | (6.8) |
| Non-BATA Funding | | 51.1 | - | 51.1 | 51.1 | 51.1 | - |
| Subtotal | | 91.3 | - | 91.3 | 84.5 | 84.5 | (6.8) |
| Project Reserves | | - | - | - | - | - | - |
| Total | | 102.1 | - | 102.1 | 96.3 | 97.1 | (5.0) |
| Richmond-San Rafael Bridge Deck Overlay Rehabilitation | | | | | | | |
| | 04152_ | | | | | | |
| Capital Outlay Support | | | | | | | |
| BATA Funding | | 4.0 | (0.4) | 3.6 | 3.3 | 3.6 | - |
| Non-BATA Funding | | 4.0 | (4.0) | - | - | - | - |
| Subtotal | | 8.0 | (4.4) | 3.6 | 3.3 | 3.6 | - |
| Capital Outlay Construction | | 16.9 | 3.6 | 20.5 | 16.3 | 16.2 | (4.3) |
| Project Reserves | | 0.1 | 0.8 | 0.9 | - | 5.2 | 4.3 |
| Total | | 25.0 | - | 25.0 | 19.6 | 25.0 | - |
| Richmond Parkway Project (RM 1 Share Only) | | | | | | | |
| | Non-Caltrans | | | | | | |
| Capital Outlay Support | | - | - | - | - | - | - |
| Capital Outlay Construction | | 5.9 | - | 5.9 | 4.3 | 5.9 | - |
| Total | | 5.9 | - | 5.9 | 4.3 | 5.9 | - |
| San Mateo-Hayward Bridge Widening | | | | | | | |
| | See note ² below | | | | | | |
| Capital Outlay Support | | 34.6 | (0.3) | 34.3 | 34.1 | 34.3 | - |
| Capital Outlay Construction | | 180.2 | - | 180.2 | 174.1 | 176.2 | (4.0) |
| Capital Outlay Right-of-Way | | 1.5 | - | 1.5 | 0.5 | 0.6 | (0.9) |
| Project Reserves | | 1.5 | 0.3 | 1.8 | - | 0.8 | (1.0) |
| Total | | 217.8 | - | 217.8 | 208.7 | 211.9 | (5.9) |
| I-880/SR-92 Interchange Reconstruction | | | | | | | |
| | EA's 23317_, 01601_, and 01602_ | | | | | | |
| Capital Outlay Support | | 28.8 | 26.2 | 55.0 | 40.8 | 55.0 | - |
| Capital Outlay Construction | | | | | | | |
| BATA Funding | | 85.2 | 60.2 | 145.4 | 29.6 | 145.4 | - |
| Non-BATA Funding | | 9.6 | - | 9.6 | - | 9.6 | - |
| Subtotal | | 94.8 | 60.2 | 155.0 | 29.6 | 155.0 | - |
| Capital Outlay Right-of-Way | | 9.9 | 7.0 | 16.9 | 11.0 | 16.9 | - |
| Project Reserves | | 0.3 | 17.8 | 18.1 | - | 18.1 | - |
| Total | | 133.8 | 111.2 | 245.0 | 81.4 | 245.0 | - |
| Bayfront Expressway Widening | | | | | | | |
| | EA's 00487_, 01511_, and 01512_ | | | | | | |
| Capital Outlay Support | | 8.6 | (0.3) | 8.3 | 8.3 | 8.2 | (0.1) |
| Capital Outlay Construction | | 26.5 | - | 26.5 | 24.9 | 26.5 | - |
| Capital Outlay Right-of-Way | | 0.2 | - | 0.2 | 0.2 | 0.2 | - |
| Project Reserves | | 0.8 | 0.3 | 1.1 | - | 1.1 | - |
| Total | | 36.1 | - | 36.1 | 33.4 | 36.0 | (0.1) |
| US 101/University Avenue Interchange Modification | | | | | | | |
| | Non-Caltrans | | | | | | |
| Capital Outlay Support | | - | - | - | - | - | - |
| Capital Outlay Construction | | 3.8 | - | 3.8 | 3.7 | 3.8 | - |
| Total | | 3.8 | - | 3.8 | 3.7 | 3.8 | - |
| Subtotal BATA Capital Outlay Support | | 358.3 | 55.0 | 413.3 | 387.7 | 412.6 | (0.7) |
| Subtotal BATA Capital Outlay Construction | | 1,569.8 | 231.0 | 1,800.8 | 1,597.8 | 1,785.8 | (15.0) |
| Subtotal Capital Outlay Right-of-Way | | 42.5 | 6.9 | 49.4 | 38.5 | 48.5 | (0.9) |
| Subtotal Non-BATA Capital Outlay Support | | 14.0 | 1.2 | 15.2 | 16.7 | 17.0 | 1.8 |
| Subtotal Non-BATA Capital Outlay Construction | | 92.4 | 9.5 | 101.9 | 82.9 | 101.9 | - |
| Project Reserves | | 35.6 | 20.2 | 55.8 | - | 50.6 | (5.2) |
| Total RM1 Program | | 2,112.6 | 323.8 | 2,436.4 | 2,123.6 | 2,416.4 | (20.0) |

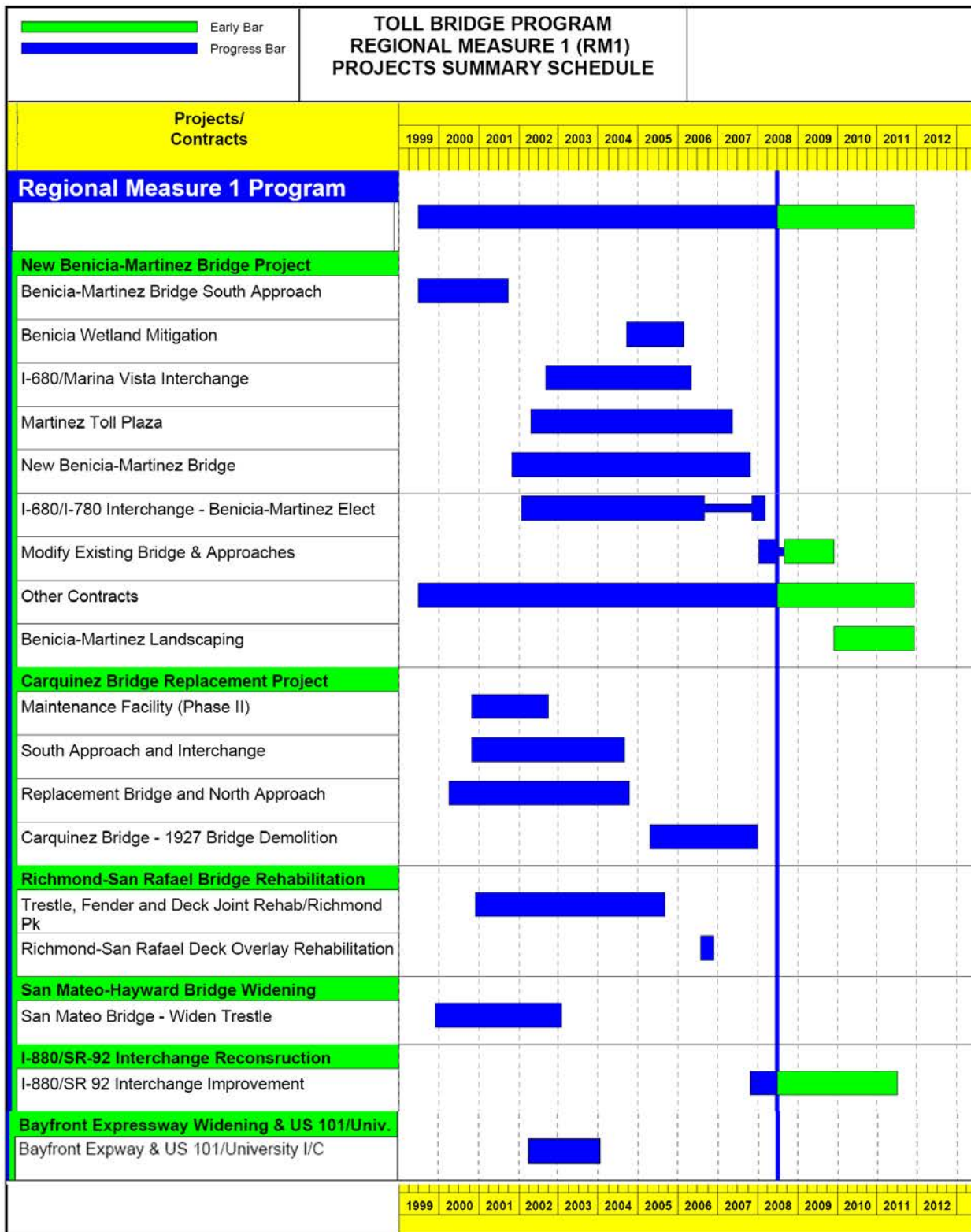
Notes:

¹ Richmond-San Rafael Bridge Trestle, Fender, and Deck Joint Rehabilitation Includes Non-TBSRA Expenses for EA 0438U_ and 04157_

² San Mateo-Hayward Bridge Widening Includes EA's 00305_, 04501_, 04502_, 04503_, 04504_, 04505_, 04506_, 04507_, 04508_, 04509_, 27740_, 27790_, 04860_

Note: Details may not sum to totals due to rounding effects.

Appendix E: Regional Measure 1 Program Summary Schedule



Appendix F: Glossary of Terms

AB144/SB 66 BUDGET: The planned allocation of resources for the Toll Bridge Seismic Retrofit Program, or subordinate projects or contracts, as provided in Assembly Bill 144 and Senate Bill 66, signed into law by Governor Schwarzenegger on July 18, 2005 and September 29, 2005, respectively.

BATA BUDGET: The planned allocation of resources for the Regional Measure 1 Program, or subordinate projects or contracts as authorized by the Bay Area Toll Authority as of June 2005.

APPROVED CHANGES: For cost, changes to the AB144/SB 66 Budget or BATA Budget as approved by the Bay Area Toll Authority Commission. For schedule, changes to the AB 144/SB 66 Project Complete Baseline approved by the Toll Bridge Program Oversight Committee, or changes to the BATA Project Complete Baseline approved by the Bay Area Toll Authority Commission.

CURRENT APPROVED BUDGET: The sum of the AB144/SB66 Budget or BATA Budget and Approved Changes.

COST TO DATE: The actual expenditures incurred by the program, project or contract as of the month and year shown.

COST FORECAST: The current forecast of all of the costs that are projected to be expended so as to complete the given scope of the program, project, or contract.

AT COMPLETION VARIANCE or VARIANCE (cost): The mathematical difference between the Cost Forecast and the Current Approved Budget.

AB 144/SB 66 PROJECT COMPLETE BASELINE: The planned completion date for the Toll Bridge Seismic Retrofit Program or subordinate projects or contracts.

BATA PROJECT COMPLETE BASELINE: The planned completion date for the Regional Measure 1 Program or subordinate projects or contracts.

PROJECT COMPLETE CURRENT APPROVED SCHEDULE: The sum of the AB144/SB66 Project Complete Baseline or BATA Project Complete Baseline and Approved Changes.

PROJECT COMPLETE SCHEDULE FORECAST: The current projected date for the completion of the program, project, or contract.

SCHEDULE VARIANCE or VARIANCE (schedule): The mathematical difference expressed in months between the Project Complete Schedule Forecast and the Project Complete Current Approved Schedule.

The following information is provided in accordance with California Government code Section 755. This document is one of a series of reports prepared for the Bay Area Toll Authority (BATA)/Metropolitan Transportation Commission (MTC) for the Toll Bridge Seismic Retrofit and Regional Measure 1 Programs. The contract value for the monitoring efforts, technical analysis, and field site works that contribute to these reports, as well as the report preparation and production, is \$1,574,873.73.

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ITEM 4: PROGRAM ISSUES

- a) Opportunity Schedule Update*

TO: Toll Bridge Program Oversight Committee (TBPOC) **DATE:** August 27, 2008

FR: Tony Anziano, Toll Bridge Program Manager, Caltrans

RE: Agenda No. - 4a

Item- Program Issues

Opportunity Schedule Update

Recommendation:

For Information Only

Cost:

N/A

Schedule Impacts:

Currently, two activities have occurred that impact the current opportunity schedule:

- 1 – the 5 month delay reported by American Bridge/Fluor on the SAS project; and
- 2 – the change in the Roll Out/Roll In date for the Yerba Buena Island Detour project from late May 2009 to early September 2009.

An updated opportunity schedule has been prepared to reflect these changes.

Discussion:

The Corridor Schedule Team's (CST) primary function is to assess and mitigate corridor schedule risks. The team, consisting of Caltrans, BATA, and CTC representatives, integrates and coordinates schedules with the project schedule team, coordinates with risk response teams, reviews opportunities to enhance the corridor schedule, and provides recommendations to management regarding schedule decisions and schedule risk mitigation.

One of the CST's outputs is the corridor Opportunity Schedule. The OS incorporates opportunities to enhance the corridor schedule and mitigate potential schedule risks. The OS is a valuable tool used for planning purposes and for the effective coordination of the various interfacing corridor contracts. The CST updates and proposes revisions to

Memorandum

the OS in light of actual progress, recent developments, current decision options, and provides recommendations to management. As a result of actual progress of the east tie in work on the YBI Detour, as well as recent developments and schedule uncertainties on the SAS contract, the CST has proposed that the current OS be updated. The updated OS will provide for the most effective planning and management of all corridor contracts.

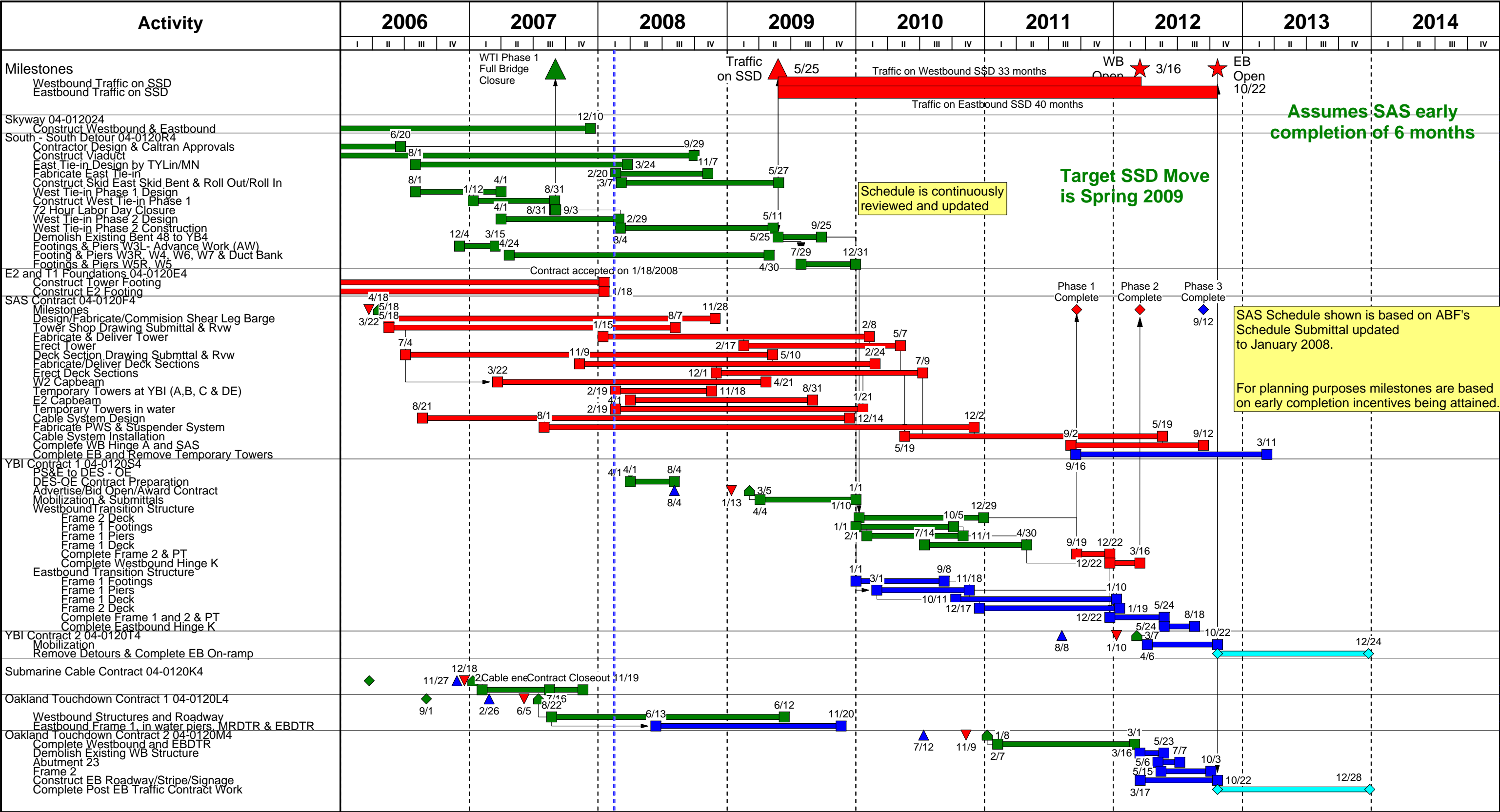
BATA and CTC CST representatives have reviewed and concur with adoption of the proposed updated OS.

Attachments:

1. Current Opportunity Schedule
2. Proposed Revised Opportunity Schedule

SFOBB - East Span Seismic Safety Project
Opportunity Schedule

Attachment #2
2/15/08



Assumes SAS early completion of 6 months

Target SSD Move is Spring 2009

Schedule is continuously reviewed and updated

SAS Schedule shown is based on ABF's Schedule Submittal updated to January 2008.

For planning purposes milestones are based on early completion incentives being attained.

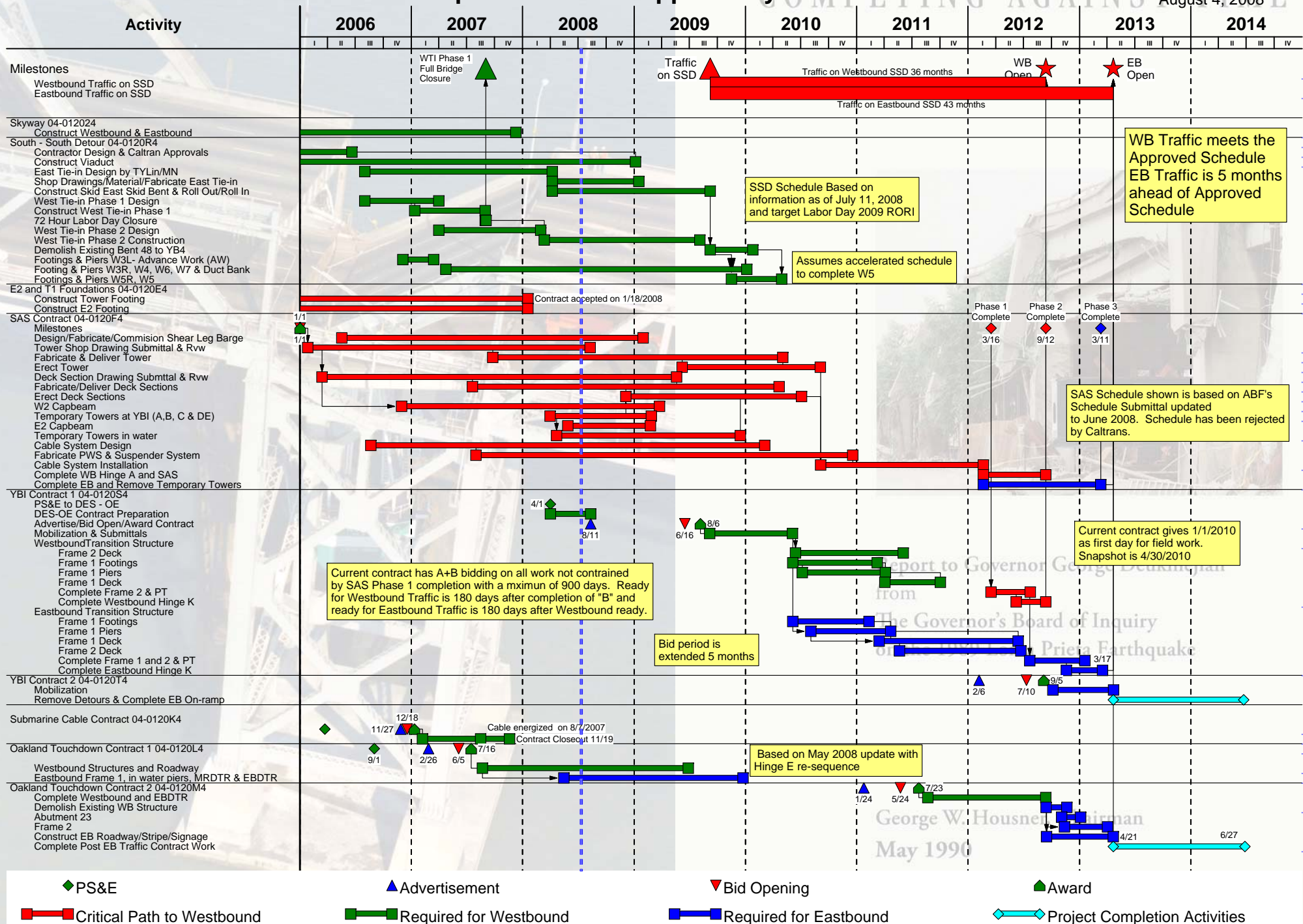
This schedule is a proposed revision to the Opportunity Schedule. It based on information current as of the July 31, 2008 and certain assumptions to accelerate portions of the work.

SFOBB - East Span Seismic Safety Project

Proposed Revised Opportunity Schedule

Draft for discussion only

August 4, 2008



ITEM 5: SAN FRANCISCO-OAKLAND BAY BRIDGE UPDATES

- a. Self-Anchored Suspension Superstructure
(SAS)
 - 1) China Update

Memorandum

TO: Toll Bridge Program Oversight Committee (TBPOC) **DATE:** August 27, 2008

FR: Tony Anziano, Toll Bridge Program Manager, Caltrans

RE: Agenda No. - 5a1

Item- San Francisco-Oakland Bay Bridge Updates
Self-Anchored Suspension Superstructure (SAS)
China Update

Recommendation:

For Information Only

Cost:

N/A

Schedule Impacts:

N/A

Discussion:

A verbal update on the status of the SAS contract in China will be provided at the meeting.

ITEM 5: SAN FRANCISCO-OAKLAND BAY BRIDGE UPDATES

- a. Self-Anchored Suspension Superstructure
(SAS)
- 2) TBPOC China Itinerary

Memorandum

TO: Toll Bridge Program Oversight Committee (TBPOC) **DATE:** August 27, 2008

FR: Stephen Maller, Deputy Director, CTC

RE: Agenda No. - 5a2

Item- San Francisco-Oakland Bay Bridge Updates
Self-Anchored Suspension Superstructure (SAS)
TBPOC China Itinerary

Recommendation:

For Information Only

Cost:

N/A

Schedule Impacts:

N/A

Discussion:

The TBPOC is scheduled to meet in Shanghai, China on Wednesday, October 1, 2008. The broad stroke itinerary for the China trip is as follows:

- Saturday, September 27, 2008 – leave San Francisco for Hong Kong.
- Sunday, September 28, 2008 – arrive Hong Kong.
- Monday, September 29, 2008 - visit Stonecutters Bridge, Hong Kong.
- Tuesday, September 30, 2008 – leave Hong Kong arrive Shanghai - meet with ABF, late afternoon or early evening.
- Wednesday, October 1, 2008 – visit ZPMC manufacturing facilities on Changxing Island – TBPOC meeting – possible ZPMC dinner in the evening.
- Thursday, October 2, 2008 – visit Sutong Bridge day 1, Changshu – lunch & dinner with Sutong Bridge Commend.
- Friday, October 3, 2008 – visit Sutong Bridge day 2, Nantong – breakfast & lunch with Sutong Bridge Commend, back to Shanghai 2:00 pm.
- Saturday, October 4, 2008 – leave Shanghai for San Francisco, Beijing or Sapporo as appropriate.

Attachments: 1) Attendees, Accommodations and Flight Information
2) Schedule for Visiting Sutong Bridge

ATTENDEES, ACCOMMODATIONS and FLIGHT INFORMATION

September/October 2008 TBPOC China Meeting

| | 9/26/2008 | 9/27/2008 | 9/28/2008 | 9/29/2008 | 9/30/2008 | 10/1/2008 | 10/2/2008 | 10/3/2008 | 10/4/2008 | 10/5/2008 | 10/6/2008 | 10/7/2008 |
|---------------------------------------------------------------------|-----------------------------------------|----------------------------------------|----------------------------|--------------------------|--------------------------------------------------|----------------------|----------------------|-----------------------------------------------|-------------------------------------------------------------|-----------------------------------------------|--------------------------|-----------------------------------------------------------|
| | Friday | Saturday | Sunday | Monday | Tuesday | Wednesday | Thursday | Friday | Saturday | Sunday | Monday | Tuesday |
| Agenda | | | | | | | | | | | | |
| Morning | | | | Stonecutter's Bridge | Travel to Shanghai | ZPMC Facilities | Travel to Sutong | Sutong Bridge | Travel to Sapporo | | Japan Steel Works | |
| Afternoon | | | | | | | Sutong Bridge | Travel to Shanghai | | | | |
| Evening | | | | | ABF Meeting | ZPMC Dinner | Sutong Dinner | | | | | |
| | | | | | | | | | | | | |
| Accommodations | | | | | | | | | | | | |
| Kowloon Shangri La Hotel, Hong Kong | Kowloon Shangri La Hotel | Kowloon Shangri La Hotel | Kowloon Shangri La Hotel | Kowloon Shangri La Hotel | | | | | | | | |
| JW Marriott Shanghai Hotel | | | | | JW Marriott Shanghai | JW Marriott Shanghai | JW Marriott Shanghai | JW Marriott Shanghai | JW Marriott Shanghai | | | |
| Suzhou Hotel, Changshu China | | | | | | | Suzhou Hotel | | | | | |
| Keio Plaza Hotel, Sapporo, Japan | | | | | | | | | Keio Plaza Hotel Sapporo | Keio Plaza Hotel Sapporo | Keio Plaza Hotel Sapporo | |
| Shangrila Kerry, Beijing | | | | | | | | | Shangrila Kerry Beijing | Shangrila Kerry Beijing | Shangrila Kerry Beijing | |
| | | | | | | | | | | | | |
| Attendees and Flight Information | | | | | | | | | | | | |
| California Department of Transportation (Caltrans) | | | | | | | | | | | | |
| Will Kempton +1 No Additional Room Director | | | | | | | | | | | | |
| Tony Anziano Program Manager | | | | | | | | | | | | |
| Ken Terpstra Project Manager | | | | | | | | | | | | |
| California Transportation Commission | | | | | | | | | | | | |
| John Barna +1 One Additional Room Executive Director | Cathay Pacific 873 LV: SFO 1:20 A | Cathay Pacific 873 AR: HK 6:25 A | | | DragonAir 876 LV: HK 10:00A AR: PVG 12:30P | | | | China Eastern 272 LV: PVG 2:10 P AR: PEK 4:35 P | | | Cathay Pacific 6111 LV: PEK 9:25 A AR: HK 1:05 P |
| Stephen Maller Deputy Director | | AA 6079 LV: SFO 1:35P | AA 6079 AR: HK 6:40P | | DragonAir 876 LV: HK 10:00A AR: PVG 12:30P | | | | China Eastern 279 LV: PVG 8:15A AR: Sapporo 12:20P | | | All Nippon 708 LV: Sapporo 12:55P AR: Nagoya 2:40P |
| Dina Noel Associate Deputy Director | | AA 6079 LV: SFO 1:35P | AA 6079 AR: HK 6:40P | | DragonAir 876 LV: HK 10:00A AR: PVG 12:30P | | | | China Eastern 279 LV: PVG 8:15A AR: Sapporo 12:20P | | | United 830 LV: Nagoya 3:45P |
| | | | | | | | | | | | | All Nippon 708 LV: Sapporo 12:55P AR: Nagoya 2:40P |
| | | | | | | | | | | | | United 830 LV: Nagoya 3:45P |
| Bay Area Toll Authority (BATA) | | | | | | | | | | | | |
| Bill Dodd Chairman | | United 869 LV: SFO 1:12P | United 869 AR: HK 6:00P | | DragonAir 876 LV: HK 10:00A AR: PVG 12:30P | | | | China Eastern 279 LV: PVG 8:15A AR: Sapporo 12:20P | | | All Nippon 708 LV: Sapporo 12:55P AR: Nagoya 2:40P |
| Steve Heminger Executive Director | | United 869 LV: SFO 1:12P | United 869 AR: HK 6:00P | | DragonAir 876 LV: HK 10:00A AR: PVG 12:30P | | | | | United 858 LV: PVG 12:25P AR: SFO 8:27A | | |
| Andrew Fremier Deputy Executive Director | | United 869 LV: SFO 1:12P | United 869 AR: HK 6:00P | | DragonAir 876 LV: HK 10:00A AR: PVG 12:30P | | | | China Eastern 279 LV: PVG 8:15A AR: Sapporo 12:20P | | | All Nippon 708 LV: Sapporo 12:55P AR: Nagoya 2:40P |
| Francis Chin, General Counsel | | In Hong Kong | | | DragonAir 876 LV: HK 10:00A AR: PVG 12:30P | | | | | United 858 LV: PVG 12:25P AR: SFO 8:27A | | |
| Peter Lee +1 No Additional Room Senior Program Coordinator | | In Hong Kong | | | DragonAir 876 LV: HK 10:00A AR: PVG 12:30P | | | United 858 LV: PVG 12:25P AR: SFO 8:27A | | | | |

Schedule for Visiting Sutong Bridge for California Transportation Mission

(October 2 – October 3, 2008)

| No. | Date | Activities | Note |
|-----|-------|-----------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|------|
| 1 | 02/10 | Shanghai—Changshu—Nantong—Suzhou 9 :00 AM Leave for Changshu 10:30AM Arriving at Sutong Bridge Co. Ltd, Changshu 10:50 AM Visiting Sutong Bridge Exhibition Hall 12:00PM Lunch 2:00PM Visiting Sutong Bridge 3:00PM Visiting Langshan Mountain, Nantong 6:00PM Dinner 9:00 PM Stay at Suzhou Hotel | |
| 2 | 03/10 | Suzhou—Shanghai 8:00AM Breakfast 9:30AM Visiting Humble Administrative Garden 12:00PM Lunch 2:00PM Back to Shanghai | |

美国加州交通运输代表团参观苏通大桥行程安排

(2008 年 10 月 2 日至 3 日)

| 序号 | 日期 | 活动内容 | 备注 |
|----|-------|---------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|----|
| 1 | 02/10 | <p>上海—常熟—南通—苏州</p> <p>9: 00 前往常熟 10: 30 抵达苏通大桥有限责任公司 10: 50 参观苏通大桥展览馆 12: 00 午餐 14: 00 参观苏通大桥 15: 00 游览南通狼山 18: 00 晚餐 21: 00 住苏州</p> | |
| 2 | 03/10 | <p>苏州 - 上海</p> <p>8: 00 早餐 9: 30 游览苏州拙政园 12: 00 午餐 14: 00 返回上海</p> | |

**ITEM 5: SAN FRANCISCO-OAKLAND BAY
BRIDGE UPDATES**

b. Yerba Buena Island Detour (YBID)

1) Update

Memorandum

TO: Toll Bridge Program Oversight Committee (TBPOC) **DATE:** August 27, 2008

FR: Tony Anziano, Toll Bridge Program Manager, Caltrans

RE: Agenda No. - 5b1

Item- San Francisco-Oakland Bay Bridge Updates

Yerba Buena Island Detour (YBID) Update

Recommendation:

For Information Only

Cost:

N/A

Schedule Impacts:

N/A

Discussion:

A verbal update on the status of the Yerba Buena Island Detour contract will be provided at the meeting.

**ITEM 5: SAN FRANCISCO-OAKLAND BAY
BRIDGE UPDATES**

b. Yerba Buena Island Detour (YBID)

2) CCO 140

Memorandum

TO: Toll Bridge Program Oversight Committee (TBPOC) **DATE:** August 27, 2008

FR: Tony Anziano, Toll Bridge Program Manager, Caltrans

RE: Agenda No. - 5b2

Item- San Francisco-Oakland Bay Bridge Updates

Yerba Buena Island Detour (YBID)

Contract Change Order 140

Recommendation:

APPROVAL

Cost:

\$10.9 million

Schedule Impacts:

N/A

Discussion:

Contract Change Order (CCO) 140 involves the steel fabrication required for the East Tie In of the Yerba Buena Island Detour. The amount required for the CCO is \$10.9 million, \$3.4 million above the last estimate of \$7.5 million. The majority of this additional amount relates to additional design requirement.

Attachments:

1. Contract Change Order 140
2. CCO 140 Memorandum
3. Contract Change Order Implementation Strategy, August 2008 Updated
4. Budget Analysis, June 30, 2008

| | | | | | |
|------------------------------|--------------|--------------------------|----------------------|-----------------------------------|-------------------------------|
| CONTRACT CHANGE ORDER | | | | | Change Requested by: Engineer |
| CCO 140 | Suppl. No. 0 | Contract No. 04 - 0120R4 | Road SF-80-12.6/13.2 | FED. AID LOC.: ACBRIM-080-1(097)N | |

To: CC MYERS INC

You are directed to make the following changes from the plans and specifications or do the following described work not included in the plans and specifications for this contract. **NOTE: This change order is not effective until approved by the Engineer.**

Description of work to be done, estimate of quantities and prices to be paid. (Segregate between additional work at contract price, agreed price and force account.) Unless otherwise stated, rates for rental of equipment cover only such time as equipment is actually used and no allowance will be made for idle time. This last percentage shown is the net accumulated increase or decrease from the original quantity in the Engineer's Estimate.

Extra Work at Lump Sum:

Perform all work shown on the plans and specifications of Pages No. 3 through 81 of this change order to provide for the steel fabrication and delivery of the steel truss of the East Tie-In (ETI) portion of the Temporary Bypass Structure (Bridge No. 34-0006 (TEMP)). All welding performed under this change order shall conform to Sheets No. 5 through 12 (Section 8 - 3._ WELDING) of Change Order No. 112.

The fabrication of the truss shall be performed in a manner that shall minimize the stresses in the structure in its permanent (post-camber) geometric configuration. This requires the fabrication of all field end holes (bolt holes) to match the permanent configuration as opposed to their cambered position. It is understood that this fabrication method shall result in considerable additional cost to the fabrication of the structure. Particularly, the shop pre-assembly process will require several stages of fit up and will require truss members to be manipulated by force in order to match the end connections. Compensation provided herein includes all additional costs resulting from this specified fabrication method.

The costs of procuring all raw steel, welding wire and fasteners for the truss are excluded from this change and shall be compensated under Change Order No. 112.

The cost of furnishing all expansion joints, elastomeric bearing pads, decking and floor beam shear studs for the truss are excluded from this change order.

The costs associated with the erection of the truss and the fabrication and erection of the ETI skid bent and beam are excluded from this change order.

The cost of transporting the fabricated steel of the truss to the project site is included in this change order. In the event the fabricated steel is required to be delivered and stored off-site, all additional storage, handling and transportation costs associated with this off-site storage shall be compensated under a separate change order.

The costs of unloading the fabricated steel at the project site are excluded from this change order and shall be compensated under a separate change order.

Total Cost of Extra Work at Agreed Lump Sum\$10,920,525.00

The lump sum payment provided herein is based on the delivery of the fabricated steel truss to the project by March 8, 2009. In the event the fabricated steel is delivered prior to that date, the Contractor shall be paid an incentive of \$10,000.00 for each calendar day prior to March 8, 2009 that the steel is delivered. In the event the fabricated steel is delivered after that date, the Contractor shall credit the Department a disincentive of \$10,000.00 for each calendar day after March 8, 2009 that the steel is delivered. Any incentive payment or disincentive credit shall not exceed \$300,000.00 and shall be provided under a supplemental change order.

For the purpose of this incentive / disincentive, the delivery of the fabricated steel shall be defined as the date sufficient steel is delivered to the project to allow for the ETI truss steel erection activities to begin. Any subsequent delays to the steel erection activities, due to the failure to deliver the remaining fabricated steel, shall result in a commensurate adjusted to the delivery date.

CONTRACT CHANGE ORDER

Change Requested by: Engineer

| | | | | |
|---------|--------------|--------------------------|----------------------|-----------------------------------|
| CCO 140 | Suppl. No. 0 | Contract No. 04 - 0120R4 | Road SF-80-12.6/13.2 | FED. AID LOC.: ACBRIM-080-1(097)N |
|---------|--------------|--------------------------|----------------------|-----------------------------------|

There shall be no reduction in compensation for check samples removed form any mill located more than 480 air line kilometers from Sacramento and Los Angeles or for additional shop inspection expenses sustained by the State for fabrication sites more 300 air line miles from both Sacramento and Los Angeles.

All payment clauses contained within the specifications of this change order are superseded by the agreed lump sum payment method specified above.

Estimated Cost: Increase ☒ Decrease ☐ \$10,920,525.00

By reason of this order the time of completion will be adjusted as follows: Deferred

| | | |
|--------------|---------------------------------|------|
| Submitted by | | |
| Signature | Resident Engineer BILL CASEY | Date |

| | | |
|-------------------------|-------------------------------------------|------|
| Approval Recommended by | | |
| Signature | SFOBB Construction Manager MIKE FORNER | Date |

| | | |
|----------------------|-------------------------------------------|------|
| Engineer Approval by | | |
| Signature | SFOBB Construction Manager MIKE FORNER | Date |

We the undersigned contractor, have given careful consideration to the change proposed and agree, if this proposal is approved, that we will provide all equipment, furnish the materials, except as may otherwise be noted above, and perform all services necessary for the work above specified, and will accept as full payment therefor the prices shown above.

NOTE: If you, the contractor, do not sign acceptance of this order, your attention is directed to the requirements of the specifications as to proceeding with the ordered work and filing a written protest within the time therein specified.

| | | |
|--------------------------|------------------------|------|
| Contractor Acceptance by | | |
| Signature | (Print name and title) | Date |

CONTRACT CHANGE ORDER MEMORANDUM

| | | | | |
|-------------------------------------------------------|--------------------------------------|--------------------------------------------------------------------------------|---------------------------------------------------------------------------------------------------------------------------------|----------------------------------------------------------------------------|
| TO: MIKE FORNER / DEANNA VILCHECK | | | FILE: E.A. 04 - 0120R4 | |
| FROM: BILL CASEY | | | CO-RTE-PM SF-80-12.6/13.2 | |
| | | | FED. NO. ACBRIM-080-1(097)N | |
| CCO#: 140 | SUPPLEMENT#: 0 | Category Code: CHPA | CONTINGENCY BALANCE (incl. this change) \$5,149,544.12 | |
| COST: \$10,920,525.00 | | INCREASE <input checked="" type="checkbox"/> DECREASE <input type="checkbox"/> | HEADQUARTERS APPROVAL REQUIRED? <input checked="" type="checkbox"/> YES <input type="checkbox"/> NO | |
| SUPPLEMENTAL FUNDS PROVIDED: \$0.00 | | | IS THIS REQUEST IN ACCORDANCE WITH ENVIRONMENTAL DOCUMENTS? <input checked="" type="checkbox"/> YES <input type="checkbox"/> NO | |
| CCO DESCRIPTION: Truss Steel Fabrication (Stinger) | | | PROJECT DESCRIPTION: CONSTRUCT ROUTE 80 TEMP BYPASS STRUCTURE | |
| Original Contract Time: 475 Day(s) | Time Adj. This Change: DEF Day(s) | Previously Approved CCO Time Adjustments: 1195 Day(s) | Percentage Time Adjusted: (including this change) 252 % | Total # of Unreconciled Deferred Time CCO(s): (including this change) 8 |

THIS CHANGE ORDER PROVIDES FOR:

the fabrication of the steel truss of the East Tie-In (ETI) structure.

This project, the Temporary Bypass Structure (TBS), was awarded in March 2004 to construct a detour that will allow for the tie in of the new east span of the San Francisco Oakland Bay Bridge to Yerba Buena Island. The TBS encompasses three main structures, the East Tie-In (ETI) to the existing bridge, the West Tie-In (WTI) to Yerba Buena Island, and the Viaduct structure between the two tie ins.

The original contract was awarded as a performance based contract with the contractor responsible for the design of the structures based upon meeting specified design criteria. The Department issued a December 14, 2006 memo entitled Strategy for South-South Detour Contract Completion which was approved by Tony Anziano (Toll Bridge Program Manager), Richard Land (Chief Engineer) and subsequently by the Toll Bridge Program Oversight Committee (TBPOC). This memo recommended that the design of the ETI structure be assumed by the Department as opposed to the as-bid performance based contractor design.

The new design of the ETI structure provides for a roll-out / roll-in concept with a new double deck steel truss span being erected adjacent to the existing span and then rolled into place after the existing span is rolled out. This change order provides for the fabrication and delivery of the steel truss that will be erected adjacent to the existing span and eventually be rolled into place.

The work encompassed under this change includes the fabrication of 1,270 metric tons of steel members and the delivery of the steel to the project site. Procurement of the raw steel, bolts, and welding wire necessary for this work shall be compensated under the previously approved Change Order No. 112 - Supplement No. 1. Costs associated with the erection of the truss and the fabrication and erection of the steel skid bent required for the roll-out / roll-in concept are excluded from this change order.

Authority to issue and approve this change order was received on June 6, 2008. After that authority was granted and prior to the execution of the change order, a conflict between the contractor's and the Department's assumed fabrication method was realized. This conflict concerned the inducement of stresses into the structure in its permanent or post camber position. The contractor assumed that these induced stresses were acceptable based on general industry practices, however, the design of the structure was based on limiting these stresses via the fabrication process.

The contractor has now been ordered to fabricate the structure in accordance with the Department's design assumptions. This fabrication method requires the member connections or bolt holes to be fabricated to match the structure's final position. The contractor had intended to place these connections to match the specified cambered position which would allow for a relatively simple one step pre-assembly of the structure at the fabrication shop. The Department-ordered fabrication method will result in the pre-assembly having to be performed over several iterations with the truss members having to be manipulated by force in order to achieve the fit up.

In addition, the original lump sum price was based on the Department's commitment to provide partially completed shop drawings to the contractor based off of 3-dimensional drawings that were being created along with the design process. It has now been determined that it will be more effective from a project scheduling perspective if the contractor creates their own shop drawings without trying to incorporate the partially completed Department issued drawings.

The changes to the scope of work pertaining to both the ordered fabrication method and shop drawings shall result in considerable added costs to the fabrication of the structure.

Compensation for the work of this change shall be paid as extra work at an agreed lump sum price of \$10,920,525.00, which shall be financed from the contract's contingency funds. A cost analysis is on file.

This change order also provides for an incentive / disincentive payment of \$10,000.00 per calendar day for the early or late delivery of the fabricated steel to the project based upon a March 9, 2009 delivery of the steel. Payment of this incentive / disincentive is limited to \$300,000.00. The ETI is currently the controlling operation on the project and the delivery of the fabricated steel could result in additional time related overhead and extended equipment costs of over \$25,000.00 per day. In addition to this, the current goal of performing the roll out / roll in of the ETI structure is set for Labor Day Weekend 2009. Failure to meet this date could result in months of project delays along with potentially delaying the entire SFOBB corridor. The incentive / disincentive clause is meant to mitigate these potential Department delays. Any payment or credit due under this incentive / disincentive shall be provided for under a supplemental change order.

The cost of storing the fabricated steel off-site and unloading the steel at the project site, including any traffic control, is excluding from this change order.

Any adjustment of contract time is deferred as the change may affect the controlling operation.

This change was requested by Mike Whiteside - YBI Coordination Engineer, per Memorandum on April 4, 2008 and concurred by Alec Melkonians - Asst. Project Manager, Hong Wong - Project Engineer, Patrick Treacy - HQ Assistant Construction Coordinator, and Lina Ellis - Structures Maintenance.

| CONCURRED BY: | | | ESTIMATE OF COST | | |
|--------------------------------|------------------------------------|--------------|---------------------------------------------------------------------------------------------------------------------|-----------------|-----------------|
| Construction Engineer: | Bill Casey, Resident Engineer | Date | ITEMS | THIS REQUEST | TOTAL TO DATE |
| Bridge Engineer: | Mike Whiteside, Toll Bridge Design | Date 7/23/08 | FORCE ACCOUNT | \$0.00 | \$0.00 |
| Project Engineer: | Hong Wong, PE | Date 5/22/08 | AGREED PRICE | \$0.00 | \$0.00 |
| Project Manager: | Alec Melkonians | Date 5/23/08 | ADJUSTMENT | \$10,920,525.00 | \$10,920,525.00 |
| FHWA Rep.: | | Date | TOTAL | \$0.00 | \$0.00 |
| Environmental: | | Date | | \$10,920,525.00 | \$10,920,525.00 |
| Other (specify): | Patrick Treacy, HQ Asst.Const.Co | Date 5/27/08 | FEDERAL PARTICIPATION | | |
| Other (specify): | Lina Ellis, Maintenance | Date 5/23/08 | <input type="checkbox"/> PARTICIPATING <input type="checkbox"/> PARTICIPATING IN PART <input type="checkbox"/> NONE | | |
| District Prior Approval By: | | Date | <input type="checkbox"/> NON-PARTICIPATING (MAINTENANCE) <input checked="" type="checkbox"/> NON-PARTICIPATING | | |
| HQ (Issue Approve) By: | Ken Darby, HQ CCO Engineer | Date | FEDERAL SEGREGATION (if more than one Funding Source or P.I.P. type) | | |
| Resident Engineer's Signature: | | Date | <input type="checkbox"/> CCO FUNDED PER CONTRACT <input type="checkbox"/> CCO FUNDED AS FOLLOWS | | |
| | | | FEDERAL FUNDING SOURCE PERCENT | | |
| | | | | | |
| | | | | | |
| | | | | | |

South-South Detour, Contract No. 04-0120R4
Contract Change Order Implementation Strategy
August 2008 Updated

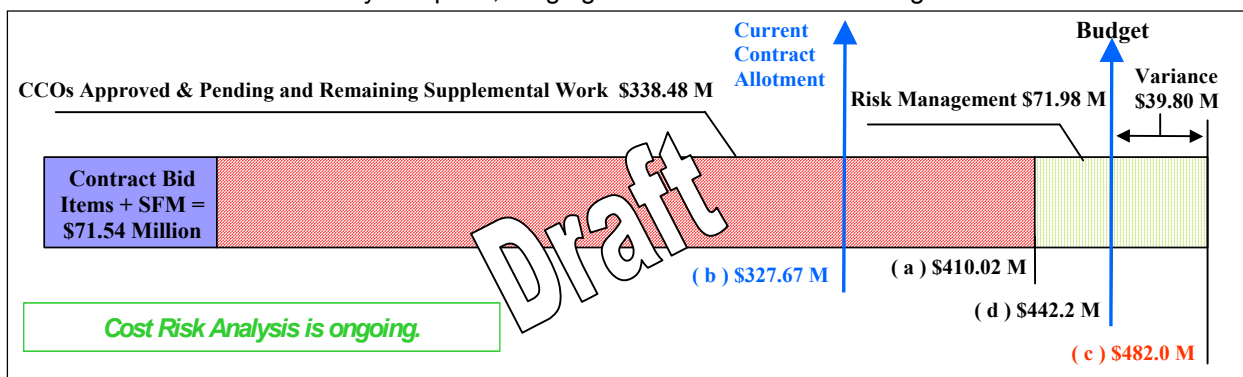
DRAFT

| South-South Detour (Contract 04-0120R4) | | | |
|-----------------------------------------|-------------------------------|--------------------------------|-------------------|
| Contract Award: | March 10 th , 2004 | Suspension Days: | 302 Working Days |
| Original Working Days: | 475 Working Days | Contract Extensions: | 1195 Working Days |
| Original Contract Completion: | July 27th, 2005 | Projected Contract Completion: | December 31, 2009 |

Introduction

Two memos were developed to outline a strategy for a revised SSD project that enhanced SSD viaduct design, developed tie-in design (east and west) in-house, improved the retrofit of the YBI viaduct (replacing the top deck of the viaduct rather than retrofitting in place) and advanced and incorporated select YBITS foundation work. The two memos are "San Francisco-Oakland Bay Bridge Corridor Schedule Mitigation – Strategy for South-South Detour Contract Completion" issued December 14, 2006, and "Recommendation to Construct Select Yerba Buena Island Transition Structure Foundations by Contract Change Order" issued on December 25, 2006. This strategy will result in substantial increases in the cost of the SSD project.

As approved at the March 2008 TBPOC meeting the revised budget for the SSD Project is 442.2M. This figure was established using available information as of January 2008 noting that the plans and specifications for the WTI Phase 2 and ETI were not fully complete, ranging from the 65% to 100% stage.



Scope of Work for SSD

The revisions to the original scope of work currently associated with the South-South Detour Project have been assigned into the following categories with their associated estimated cost:

| Category | Scope of Work | Current Budget (March 2008) | In Progress Status Update from March 08 Approved Budget | |
|------------------------|---------------------------------------------------------------------------------|-----------------------------|---------------------------------------------------------|---------------|
| | | | Current | Delta |
| (0) | Original Bid Items, Baseline CCOs (1 through 48), and State Furnished Materials | \$83.7 | \$83.7 | \$0 |
| (1) | SSD New Viaduct | \$31.9 | \$34.3 | \$2.4 |
| (2a) | West Tie-In Existing Viaduct Phase 1 | \$39.6 | \$40.0 | \$0.4 |
| (2b) | West Tie-In Phase 2 | \$15.0 | \$16.0 | \$1.0 |
| (3) | East Tie-In | \$72.5 | \$85.2 | \$12.7 |
| (4) | YBI Transition Structures Advance Foundations | \$105.8 | \$103.0 | (\$2.8) |
| (5) | Administrative Issues and General CCOs | \$48.6 | \$51.2 | \$2.6 |
| Subtotal | | \$397.1 | \$413.4 | \$16.3 |
| Contingency | | \$45.1 | \$28.8 | |
| Approved Budget | | \$442.2 | | |

Contract payments as of August 20, 2008: \$220.9M

As shown, the current status of CCOs required to modify the original scope of the SSD work as defined in Categories 1 through 5 is \$329.7 M. The status of each category of work is discussed in the succeeding pages of this report.

South-South Detour, Contract No. 04-0120R4
Contract Change Order Implementation Strategy
August 2008 Updated

DRAFT

Bid Items, Baseline CCOs, & State Furnished Material

0

The break down of Category (0) is as follows:

| | |
|------------------------------|------------------------|
| Original Contract Amount | \$ 71.2 million |
| Baseline CCOs (1 through 48) | \$ 12.1 million |
| State Furnished Materials | \$ 0.4 million |
| Total | \$ 83.7 million |

Baseline Contract Change Orders (1 through 48)

| CCO # | Description | Executed Date | Cost |
|--------------------------------------------------|----------------------------------------------------|---------------|------------------------|
| 1 | Flagging and Traffic Control | 5/13/2004 | \$100,000.00 |
| 1S1 | Additional Funds for Flagging and Traffic Control | 2/9/2007 | \$200,000.00 |
| 2 | Bidder Compensation | 5/8/2004 | \$1,575,000.00 |
| 3 | Partnering | 9/7/2004 | \$25,000.00 |
| 4 | DRB | 9/7/2004 | \$100,000.00 |
| 5 | Federal Trainee Program | 11/12/2004 | \$20,000.00 |
| 5S1 | Non-Journey Person Training | 3/10/2005 | \$50,000.00 |
| 6 | Removal of DBE/SBE Monitoring | 2/10/2005 | \$0.00 |
| 7 | Sampling and Analysis Work | 8/30/2004 | \$30,000.00 |
| 8 | SWPPP Maintenance Sharing | 8/30/2004 | \$75,000.00 |
| 9 | Additional Photo Survey/Public Relations | 9/14/2004 | \$50,000.00 |
| 10 | Temporary Shuttle Van Service | 7/16/2004 | \$650,000.00 |
| 10S1 | Additional Funds for Temporary Shuttle Van Service | 6/23/2005 | \$100,000.00 |
| 10S2 | Additional Funds for Temporary Shuttle Van Service | 1/12/2007 | \$500,000.00 |
| 11 | Utility Potholing | 9/14/2004 | \$100,000.00 |
| 12 | Just-In-Time Training (RSC Pavement) | 2/10/2005 | \$5,000.00 |
| 13 | PMIV Document Management System | 11/3/2004 | \$486,743.50 |
| 14 | Temporary Suspension | 5/19/2004 | \$0.00 |
| 15 | Archaeology Investigation | 7/19/2004 | \$30,000.00 |
| 15S1 | Additional Funds for Archaeology Investigation | 4/22/2005 | \$15,000.00 |
| 16 | Roadway Profile at WTI | Voided | N/A |
| 17 | Modify Drainage at G4 Entry Vault | 10/24/2006 | \$108,217.45 |
| 18 | Access Control Measures | 9/8/2004 | \$50,000.00 |
| 19 | EDR1 Alignment Modification | 5/12/2005 | \$0.00 |
| 20 | A490 Bolts | 10/23/2006 | \$0.00 |
| 21 | Removal /Disposal of Stairway | 4/13/2005 | \$14,060.00 |
| 22 | Clean Stairs and Walkways | 5/24/2005 | \$35,000.00 |
| 23 | Shared Field Data System (ShareArchive) | Voided | N/A |
| 24 | East and West Tie-In Temporary Suspension | 2/1/2005 | \$2,181,467.40 |
| 24S1 | Read Inclinometer/Adjust Equipment Costs | 10/18/2005 | \$29,782.99 |
| Total for Baseline Contract Change Orders | | | \$12,082,527.26 |

| CCO # | Description | Executed Date | Cost |
|-------|-----------------------------------------------|---------------|------------------------|
| 24S2 | Temporary Suspension Partially Extended | 5/2/2006 | \$4,812,631.58 |
| 24S3 | Contract Days Extension/TRO Compensation | Voided | N/A |
| 25 | Bent 48, 49R, 52R Outside Boundary | 3/24/2005 | (\$19,000.00) |
| 26 | Bent 48 Articulation | 4/22/2005 | \$0.00 |
| 27 | Bent 52L Footing Conflict | 1/19/2006 | \$94,386.51 |
| 28 | Hydroseed Around W2 Columns | 3/24/2005 | \$20,000.00 |
| 29 | Replacement of Surveillance Camera | 3/24/2005 | \$3,542.00 |
| 30 | Additional Elastic Response Analysis | 5/31/2005 | \$10,700.00 |
| 31 | Soil Analysis Outside Plan Limits | 6/27/2005 | \$20,000.00 |
| 32 | SFPUC Permit Specification Change | 5/17/2005 | \$0.00 |
| 33 | Design Enhancements | Voided | N/A |
| 34 | Pole Structure Welding Specification Revision | 9/30/2005 | \$0.00 |
| 35 | Revision of East Tie-In Design Criteria | Voided | N/A |
| 36* | Extend Limits of Viaduct Demolition | Voided | N/A |
| 37 | 4 Hr Emergency Travel Way | Voided | N/A |
| 37S1 | Emergency Travel Way Falsework | Voided | N/A |
| 38 | Revision of West Tie-In Design Criteria | 8/4/2005 | \$0.00 |
| 39 | Provide Shuttle Service to USCG | 6/27/2005 | \$10,000.00 |
| 40 | Sewer Pipe Material Change | 9/26/2005 | \$1,561.95 |
| 41 | Bent 49L Utility Relocation | Voided | N/A |
| 42 | Bent 48R Pile Load Test | 9/12/2005 | \$20,000.00 |
| 42S1 | Bent 52R Pile Load Test | 12/15/2005 | \$5,000.00 |
| 43 | Material On Hand Specification Change | 9/16/2005 | \$75,953.88 |
| 43S1 | Addition of YBITS Advance to Material On Hand | Voided | N/A |
| 44 | Electrical Call Box Relocation | | \$47,480 |
| 45 | Additional SWPPP | 2/21/2006 | \$250,000.00 |
| 46 | Southgate Road Reopening | 3/8/2006 | \$50,000.00 |
| 47 | Hazardous/Non-Hazardous Soil Removal | 12/15/2005 | \$100,000.00 |
| 48 | Buried Man-Made Objects | 12/15/2005 | \$50,000.00 |
| | | | |
| | | | \$12,082,527.26 |

- The scope of work for CCO No. 36 was completed and compensated for under the larger scope of CCO No. 76.

SSD New Viaduct

1

Progress of Work

Construction of foundations, columns, and bent caps is complete. Fabrication of the structural steel truss, performed by Dongkuk S&C in South Korea, is complete with all steel having arrived in the U.S. Concrete has been poured for both upper and lower decks in span 48. Deck construction is ongoing in Span 49, while steel erection is ongoing in Span 50. Pile driving for the temporary erection towers at Span 51 has begun.

Status of Contract Change Orders: SSD New Viaduct:

| CCO | Method of Payment | Description | HQ Status | TBPOC Status | CCO Status | Current Estimate/ Actual Cost | Change from March 08 Approved Budget |
|------------------------------------|-------------------|-----------------------------------------------------------------------------|--------------|------------------|---------------------|-------------------------------|--------------------------------------|
| 49 | LS | Stringer and Floor Beam Design Study | N/A | N/A | Executed 5/2/2006 | \$109,182 | N/A |
| 49S1 | FA | Truss Design Modifications (Changes to Stringer and Floor Beam Connections) | I&A 12/08/06 | N/A | Executed 8/17/2006 | \$150,000 | N/A |
| 49S2 | FA | | I&A 12/08/06 | N/A | Executed 12/18/2006 | \$100,000 | N/A |
| Subtotal (CCO #49 and Supplements) | | | | | | \$359,182 | |
| 50 | FA | Stand Alone Viaduct Design | N/A | N/A | Executed 5/8/2006 | \$325,000 | N/A |
| 50S1 | FA | | I&A 9/21/06 | N/A | Executed 10/16/2006 | \$300,000 | N/A |
| 50S2 | FA | | I&A 12/08/06 | N/A | Executed 12/18/2006 | \$100,000 | N/A |
| 50S3 | FA | | I&A 2/09/07 | N/A | Executed 2/13/07 | \$175,000 | N/A |
| Subtotal (CCO #50 and Supplements) | | | | | | \$900,000 | |
| 54 | LS | Deck Drainage | N/A | N/A | Executed 5/2/07 | \$8,000 | N/A |
| 55 | LS | Viaduct Fabricator Change (SGT Closeout) | I&A 7/08/07 | Approved 6/27/07 | Executed 8/7/07 | \$5,665,330 | N/A |
| 55S1 | LS | SGT Fabrication Closeout - Dongkuk Materials | | Approved 3/5/08 | Executed 3/17/08 | \$980,600 | \$70,600 |
| 59 | LS | Water Blast Rebar Cages | N/A | N/A | Executed 2/22/07 | \$5,000 | N/A |
| 60 | LS | Construction of Bent Caps | I&A 6/13/07 | Approved 6/27/07 | Executed 6/18/07 | \$7,435,950 | N/A |
| 67 | FA | Viaduct/ETI Interface Modifications (Design Cost) | I&A 5/14/07 | N/A | Executed 9/27/07 | \$800,000 | N/A |
| 79 | LS | Fabrication Cost for Viaduct Design Changes July '05 - October '06 | I&A 7/19/07 | N/A | Executed 8/7/07 | \$803,400 | N/A |
| 79S1 | LS | Fabrication Cost for Viaduct Design Changes - July 05-Oct 06 | | N/A | Executed 8/4/08 | \$75,860 | (\$174,140) |
| 80 | LS | Erection Costs for Viaduct Design Changes through October 2006 | | Approved 1/31/08 | Executed 2/20/08 | \$6,912,200 | N/A |
| 82 | FA | AC Paving and Erosion Control for Deck Drainage | | N/A | In progress | \$250,000 | \$0 |
| 85 | LS | Design of 300mm Waterline Relocation | N/A | N/A | Executed 3/17/08 | \$12,480 | \$1,994 |
| 87 | LS | Viaduct Shipping Escalation Costs | I&A 7/24/07 | N/A | Executed 10/2/07 | \$534,570 | N/A |
| 87S1 | LS | Viaduct Shipping Escalation Costs | I&A 1/14/08 | N/A | Executed 1/30/08 | \$200,000 | N/A |
| 88 | LS | Viaduct Fabrication Delays | I&A 7/19/07 | N/A | Executed 8/7/07 | \$954,460 | N/A |
| 88S1 | LS | Viaduct Fabrication Delays | I&A 8/22/07 | N/A | Executed 9/27/07 | \$776,630 | N/A |
| 98 | FA/LS | Viaduct Steel Storage and Handling Cost | | N/A | Executed 6/18/08 | \$845,370 | \$345,370 |

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|-------------------------------------------|--------------|---------------------------------------------------------------------------------|-------------|-----------------|--------------------|---------------------|--------------------|
| 99 | LS | Viaduct Erection Costs (Post Oct. 2006) | | N/A | Executed 5/22/08 | \$862,614 | (\$139,716) |
| 100 | FA | Viaduct Fabrication Costs (Post Oct. 2006) | I&A 1/22/08 | N/A | Executed 1/28/08 | \$650,000 | N/A |
| 105 | FA/LS | Dongkuk Fabrication and Temp Bracing Fabrication Costs (July 2007 Plans) | | Approved 4/3/08 | Executed 4/17/08 | \$2,140,640 | \$690,640 |
| 106 | | CCO Voided...previous scope of work was incorporated into CCO 105 | | | | - | - |
| 107 | LS | CCM Erection Support & Escalation Costs | | | In progress | \$500,000 | \$0 |
| 111 | FA/LS | USCG Parking Replacement and Protection | N/A | N/A | Executed 3/17/08 | \$163,223 | \$163,223 |
| 111S1 | LS | Additional costs USCG Parking Lot | N/A | N/A | Executed 6/30/08 | \$8,940 | \$8,940 |
| 115 | FA | Third VIA Shipping for CCO #67 July 07 plans | | N/A | Executed 5/22/08 | \$850,000 | \$450,000 |
| 128 | | Waterline Relocation (NOPC 6) | | N/A | In progress | \$200,000 | \$200,000 |
| 135 | | Deck Escalation Costs | | N/A | In progress | \$500,000 | \$0 |
| 136 | FA/LS | Relocate USCG road for steel erection FW Towers at Span 51 | N/A | N/A | In progress | \$150,000 | \$0 |
| 138 | LS | Waterline Relocation for Fire Hydrant (Conflicts with Span 49 Falsework) | N/A | N/A | In progress | \$278,200 | \$278,200 |
| 148 | FA | USCG Road Canopy below Viaduct | | N/A | In progress | \$500,000 | \$500,000 |
| Current Status for SSD New Viaduct | | | | | | \$34,322,649 | \$2,395,111 |

Budget Status

The Viaduct portion of the SSD was bid at \$26.74M. The projected additional costs in the December 14, 2006 Strategy Memorandum were estimated to be \$9M. The January 2008 revised additional cost estimate is \$31.9M with a current projection of \$34.3M. CCOs executed to date are \$31.9M.

West Tie-In

Phase 1

2a

Progress of Work

Phase 1 work was substantially complete with the move in of the Structure on September 03, 2007. Miscellaneous electrical and drainage work remain. WB On-ramp was reopened on August 8, 2008.

Status of Contract Change Orders: West Tie-In Existing Viaduct (Phase 1)

| CCO | Method of Payment | Description | HQ Status | TBPOC Status | CCO Status | Current Estimate/ Actual Cost | Change from March 08 approved Budget |
|-------|-------------------|------------------------------------------------------------------------------------------------------------------------------------------------------|--------------|------------------|-------------------|-------------------------------|--------------------------------------|
| 58 | FA | Bridge Removal Plan | N/A | N/A | Executed 11/21/06 | \$60,000 | N/A |
| 58 S1 | FA | Bridge Removal Plan | N/A | N/A | Executed 7/05/07 | \$40,000 | N/A |
| 61 | FA | Advance Engineering (Work Plans and Submittals), Site Prep (Ramp Closures, Access Road), Civil Work (Grading), Structure Work (Material Procurement) | I&A 1/09/07 | N/A | Executed 2/27/07 | \$400,000 | N/A |
| 61S1 | LS/FA | Construction of Stage 1 Area and Substructure | I&A 5/16/07 | Approved 6/27/07 | Executed 5/18/07 | \$9,995,644 | N/A |
| 66 | FA | TMP - Video Equipment (WTI Phase 1) | N/A | N/A | Executed 7/20/07 | \$175,000 | N/A |
| 68 | FA | Temporary Electrical Work | N/A | N/A | Executed 7/20/07 | \$140,000 | N/A |
| 68S1 | FA | Temporary Electrical Work Stage 2, 3 & 4 | I&A 12/02/07 | N/A | Executed 10/31/07 | \$510,000 | N/A |

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|-------------------------------------------------|----|---------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|-------------|------------------|--------------------|---------------------|------------------|
| 72 | LS | Structure Work (Superstructure), and Temporary Shuttle Service | I&A 7/19/07 | Approved 7/27/07 | Executed 7/20/07 | \$11,096,900 | N/A |
| 76 | LS | Labor Day Bridge Demolition and Move-In | I&A 7/19/07 | Approved 7/27/07 | Executed 7/20/07 | \$2,240,300 | N/A |
| 76S1 | LS | Labor Day Bridge Move-In (Changeable Message Signs, Temporary Signs, Traffic Control, Bridge Removal, Bridge Move-In, Paving and Roadway Repairs, CCM Support Costs, City Traffic Officers) | I&A 8/28/07 | Approved 8/24/07 | Executed 9/27/07 | \$10,144,140 | N/A |
| 84 | LS | Skid Track Foundations and Temporary Columns | I&A 7/27/07 | Approved 7/27/07 | Executed 7/31/07 | \$3,980,000 | N/A |
| 101 | LS | Reconstruct Slab, West Bound On-ramp | | N/A | Executed 4/17/08 | \$846,140 | \$331,140 |
| 102 | FA | Northside Drainage Work | N/A | N/A | Executed 4/4/08 | \$60,000 | \$60,000 |
| 117 | FA | Surface Drainage (Southside) | | N/A | In Progress | \$100,000 | |
| 103 | LS | Labor Day Weekend Closure Misc. Costs | | N/A | Executed 2/20/08 | \$173,140 | (\$26,860) |
| Current Status for West Tie-In (Phase 1) | | | | | | \$39,961,264 | \$364,280 |

Budget Status

The projected additional costs in the December 14, 2006 Strategy Memorandum were estimated to be \$40M. The January 2008 revised additional cost estimate is \$39.6M with a current forecast of \$40M. CCOs executed to date are \$39.9M.

West Tie-In

Phase 2

2b

Progress of Work

Construction/Design coordination meetings with the Contractor are ongoing as needed. Foundation work, which began in March 2008, is progressing on schedule. Footings and columns are complete for footings 1 through 8. Work is ongoing at footing for Bents 46 & 47.

Status of Contract Change Orders: West Tie-In (Phase 2)

| CCO | Method of Payment | Description | HQ Status | TBPOC Status | CCO Status | Current Estimate/ Actual Cost | Change from March 08 Approved Budget |
|-------------------------------------------------|-------------------|----------------------------------------------------------------------------------------|-------------|------------------|--------------------|-------------------------------|--------------------------------------|
| 62 | LS | Construction of Phase 2 Foundations and Credits for Elimination of Bid Items 12 and 90 | | Approved 4/4/08 | Executed 4/7/08 | (\$4,649,850) | \$309,150 |
| 71 | LS | WTI Phase 2 Pile at Bent 46L/Slab Bridge Removal | I&A 7/24/07 | N/A | Executed 7/20/07 | \$384,130 | N/A |
| 108 | LS | Substructure | | Approved 6/18/08 | Executed 6/25/08 | \$5,378,800 | \$720,800 |
| 141 | | Superstructure Construction | | TBD | In Progress | \$9,345,000 | \$0 |
| 143 | | Civil Work (EB Onramp and Mainline) | | TBD | In Progress | \$5,587,000 | \$0 |
| Current Status for West Tie-In (Phase 2) | | | | | | \$16,045,080 | \$1,029,950 |

Budget Status

The Contractor's bid price for the West Tie-In was \$9.0M. Based on the Department's December 14, 2006 Strategy Memorandum, the costs associated with the Phase 2 West Tie-In work were estimated to be an additional \$13.0M. The January 2008 revised additional cost estimate is \$15.0M with a current projection of \$16.0M. The January 2008 revision was based on complete foundation plans and 65% in progress substructure and superstructure plans. CCOs executed to date are \$1.1M.

East Tie-In

3

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Progress of Work

Complete bent 52A and skid bent foundations design packages were delivered October 2007. Complete ETI design plans for the skid bents and skid beams were delivered March 15th and complete truss plans were delivered April 7th. Construction/Design Coordination meetings with the Contractor are ongoing.

Fabrication subcontractors are continuing to procure material and starting fabrication work. Fabrication of the skid bent and skid beams is taking place at Thompson Metal Fab, Inc. in Vancouver, WA and the fabrication of the truss is taking place at Stinger Welding Inc. in Coolidge, AZ.

The existing SFPUC sanitary sewer pump station has been relocated, the new pump station is up and running. Construction of the skid bent foundations is progressing on schedule. Lead abatement in span YB-4 of the existing truss is complete. Work on footing and bent 52A has begun.

Status of Contract Change Orders: East Tie-In

| CCO | Method of Payment | Description | HQ Status | TBPOC Status | CCO Status | Current Estimate/ Actual Cost | Change from March 08 Approved Budget |
|---------------------------------------|-------------------|------------------------------------------------------------------------------------------------------------------------|--------------|------------------|--------------------|-------------------------------|--------------------------------------|
| 63 | FA | Advance Engineering (Work Plans and Submittals) | I&A 8/22/07 | N/A | Executed 9/27/07 | \$800,000 | N/A |
| 69 | LS | Procurement of Pump/Control Panel for Pump Station Relocation | N/A | N/A | Executed 10/10/07 | \$111,280 | N/A |
| 69S1 | LS | Construction for Pump and Control Panel for Relocated Pump Station | | N/A | Executed 3/17/08 | \$499,996 | \$11,986 |
| 90 | LS | Bent 52A and Skid Bent Footings and Credits for Eliminated Bid Items 10 and 42 | | Approved 4/4/08 | Executed 4/14/08 | \$11,308,380 | \$0 |
| 92 | FA | ETI AT&T Fiber Optic Relocation | N/A | N/A | Executed 12/17/07 | \$175,000 | N/A |
| 93 | FA | Lead Paint Mitigation Existing Truss | | N/A | Executed 2/20/08 | \$563,725 | \$3,725 |
| 97 | FA | Bent 52A and Skid Bent Ftg's Material Procurement | I&A 11/06/07 | N/A | Executed 11/19/07 | \$850,000 | N/A |
| 104 | LS | Pier E-1 Access Towers | N/A | N/A | Executed 1/30/08 | \$150,000 | N/A |
| 113 | LS | Relocate Waterline in Conflict with Northern Skid Bent Footings | N/A | N/A | Executed 3/17/08 | \$167,990 | \$167,990 |
| 121 | LS | Soil Nail Wall Material Procure | N/A | N/A | Executed 3/17/08 | \$142,670 | N/A |
| 127 | | RTU - 8 Service Platform | N/A | N/A | In Progress | \$150,000 | \$0 |
| 129 | | Erection, Roll-In Roll-Out, Joint Seals, Demolition, Existing Truss Retrofit, Stage 2 Wall, TMP, and Civil Work | | | In Progress | \$30,189,500 | \$0 |
| 137 | LS | Pump station Water Tank Demo | N/A | N/A | Executed 6/26/08 | \$114,490 | \$114,490 |
| 112 | FA | Material Procure Skidbent (1532 Tower Legs) | | Approved 2/4/08 | Executed 2/19/08 | \$2,000,000 | \$12,363,705 |
| 112S1 | FA | Material Procure ETI Superstructure | | Approved 3/5/08 | Executed 3/17/08 | \$8,500,000 | |
| 112S2 | FA | Material Procure ETI Temporary Bypass Structure | | | In Progress | \$3,500,000 | |
| 116 | FA/LS | Fabricate Superstructure & Skidbent | | Approved 6/16/08 | Executed 8/8/08 | \$14,166,180 | |
| 140 | LS | Truss Steel Fabrication | | At TBPOC | In Progress | \$10,920,525 | \$0 |
| 144 | FA | Expansion Joint Mock-up | | N/A | In Progress | \$850,000 | |
| Current Status for East Tie-In | | | | | | \$85,159,736 | \$12,661,896 |

Budget Status

The Contractor's bid price to construct the Contractor's design for the East Tie-In was \$6.0M with an additional \$1.46M to demolish the remaining portion of the ETI YB-4 span. The Department's December 14, 2006

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Strategy Memorandum estimated an additional cost of \$34.0M to construct the Department's ETI roll out/roll in design concept. At the time, this estimate was based on minimal design information available. The January 2008 revised additional cost estimate is \$72.5M, with the current projection at \$85.2M. The January 2008 revision was based on complete Bent 52A and skid bent foundation design plans and 65% skid bent, skid beam, and truss design plans. CCOs executed to date are \$39.5M.

The material procurement and fabrication cost increases (CCOs 112, 116, & 140) are attributed to an increase in steel weight from the 65% to 100% designed plans along with a market fluctuation in steel price as well as additional costs to expedite the Steel Truss fabrication work.

Yerba Buena Island Transition Structures
Advance Foundations

4

Progress of Work

The YBITS foundation and column locations being advanced are W3R/L, W4R/L, W5R/L, W6R/L, W7R/L, W7 Ramp and the temporary E.B. onramp abutment.

- W3 3L – substantially completed
3R – excavation for footing is completed
 - W4 4L – substantially completed
4R – column (2nd lift of 3) in progress
 - W5 5L – 75 of 140 piles driven
5R – work not started
 - W6 6L – column (3rd lift of 3) in progress
6R North - column (2nd lift of 3) in progress
6R South - work not started.
 - W7 Mainline – construction of the temporary soil nail wall in progress
Ramp – work not started.
- EB on-ramp abutment – work not started.

Status of Contract Change Orders: YBI Transition Structures Advance Foundations

| CCO | Method of Payment | Description | HQ Status | TBPOC Status | CCO Status | Current Estimate/ Actual Cost | Change from March 08 Approved Budget |
|-------------|-------------------|--------------------------------------------------------------------------------------------------------------------------------------------|--------------|-------------------|--------------------|-------------------------------|--------------------------------------|
| 64 | FA | YBITS W3L Site Prep and Grading and Construct Access Road | N/A | N/A | Executed 1/8/07 | \$150,000 | N/A |
| 64S1 | LS/FA | YBITS W3L Foundation and Column to Splice Zone, Integrated Shop Drawings for W3L, Concrete Washouts, 50% of Flagging, and Traffic Controls | I&A 3/13/07 | Approved 2/15/07 | Executed 4/4/07 | \$5,835,000 | N/A |
| 65 | FA | Demo Exist Bridge Adv. Planning | N/A | Approved 4/14/08 | Executed 4/18/08 | \$175,000 | \$0 |
| 65S1 | | Demolish Exist Bridge (Bent 48 to YB-4) | | TBD | In Progress | \$7,625,000 | \$0 |
| 70 | FA | Integrated Shop Drawings for Remaining YBITS Advance Locations (W3R, W4L/R, W5L/R, W6L/R, W7L/R, and W7 Ramp) | I&A 4/04/07 | N/A | Executed 5/1/07 | \$500,000 | N/A |
| 70S1 | FA | YBITS Advance – ISD 3R, 4R/L, 5R/L, 6R/L, 7R/L & ramp | | N/A | Executed 1/30/08 | \$450,000 | N/A |
| 73 | LS | YBITS W3R, W4R, W5R/L, W6R/L, and W7 Ramp Foundations and Columns | I&A 10/24/07 | Approved 10/30/07 | Executed 11/19/07 | \$62,958,990 | N/A |
| 73S1 | | Duct Bank Revisions | | N/A | In Progress | \$200,000 | \$200,000 |
| 75 | LS | YBITS W7R/L Foundations and Columns | | Approved 4/3/08 | Executed 4/14/08 | \$13,150,000 | (\$3,657,884) |
| 75S1 | | Bent W7 Structure Backfill | | | In Progress | \$1,750,000 | |
| 77 | LS | YBITS W4L Foundations and Columns | I&A 6/13/07 | Approved 7/27/07 | Executed 7/20/07 | \$7,125,000 | N/A |
| 78 | FA | Relocation of Sewer Force Main | N/A | N/A | Executed 7/17/07 | \$125,057 | N/A |
| 94 | LS | YBITS Temp. EB Onramp Abutment and Staging | | TBD | In Progress | \$2,219,850 | \$0 |

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|-------------------------------------------------------------------------|-----------------|--------------------------------------------------|--|--------------------|---------------------|----------------------|-----------------------|
| 118 | FA | Vibration & Elev. Monitoring at W5L | | N/A | Executed 2/20/08 | \$50,000 | \$50,000 |
| 118S1 | FA/LS/ID | Nimitz House vibration monitoring | | N/A | In Progress | \$50,000 | \$50,000 |
| 120 | LS/Credit | CIDH Pile Mitigation Deduct | | N/A | Executed 3/17/08 | (\$400) | (\$400) |
| 124 | | Seismic Monitoring & Column Grounding | | N/A | In Progress | \$100,000 | \$100,000 |
| 126 | FA | YBITS Excavation / Hazmat Disposal | | Approved 4/3/08 | Executed 4/17/08 | \$500,000 | \$400,000 |
| 147 | LS | Add Cost W4R Foundation Construction | | N/A | Executed 7/21/08 | \$25,024 | \$25,024 |
| Current Status for YBI Transition Structures Advance Foundations | | | | | | \$102,988,521 | (2,833,260.00) |

Budget Status

The Department's December 25, 2006 Strategy Memorandum estimated the cost to construct Bents W3R/L, W4R/L, W5R/L, W6R/L, W7R/L, and W7 Ramp to be \$107M. In addition, the temporary E.B. onramp abutment was added at a later date with no estimate revision. The Departments December 14, 2006 Strategy Memorandum estimated the additional demolition costs for the existing bridge (Bent 48 through YB-4) to be \$3.5M. Removal of the existing bridge is included in the current contract; however, the Department anticipates additional costs resulting from impacts of the YBITS Advance work and associated costs due to escalation. The combined estimate for both was \$110.5M. The January 2008 revised additional cost estimate is \$105.8M with a current projection at \$103M. CCOs executed to date are \$91M.

Administrative Issues General CCOs

5

Progress of Work

Administrative issues that remain on the SSD contract are related to setting project milestones and determining time related overhead resulting from the contract time extensions, escalation costs, the increased scope of work, and other necessary changes to the contract. Additionally, costs for implementing COZEPP for the East and West Tie-Ins need to be accounted for.

The following list of target milestones was previously provided to the Contractor to incorporate into the project schedule. This information will be revised as more detailed schedule information is developed.

| | Date | Status | Notes |
|---------------------------------------------------|---------------------|-------------------------|------------------------------|
| W3L (foundation and column up to splice zone) | March 15th, 2007 | Complete | finished 3/15/07 |
| West Tie-In Phase 1 Viaduct Demo/Roll-In Complete | September 4th, 2007 | Complete | finished 9/04/07 |
| Access to W3R Available to CCM | January 2nd, 2008 | Partial access provided | coordinating access with SAS |
| W3R, W4L/R, W6L/R, and W7L/R/Ramp Complete | December 31st, 2008 | | |
| Upper East Tie-In Area Available to CCM | April 2nd, 2009 | Partial access provided | coordinating access with SAS |
| East Tie-In Roll-Out/Roll-In Complete | May 26th, 2009 | | |
| Frame 1 YBITS Area (Bent 7 West) Vacated by CCM | September 1st, 2009 | | |
| Project Completion | December 31st, 2009 | | |

The Department has extended TRO compensation at the original contract rate through September 1, 2009. The Contractor has completed a TRO audit. The Department is reviewing this information so that an appropriate TRO adjustment can be negotiated.

The Department continues to pursue a resolution to the remaining NOPC issues. Of the 18 NOPC issues, only three remain outstanding. Of the three it is anticipated that Viaduct CCO #128 will resolve NOPC #6, resolution of the existing structure demolition costs will resolve NOPC #15, and resolution of the TRO costs will resolve NOPC #18.

Status of Contract Change Orders: Administrative Issues

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| CCO | Method of Payment | Description | HQ Status | TBPOC Status | CCO Status | Current Estimate/ Actual Cost | Change from March 08 Approved Budget |
|-----------------------------------------------------------|-------------------|-----------------------------------------------------------------------------------|--------------|-------------------|--------------------|-------------------------------|--------------------------------------|
| 1 S2 | FA | Flagging & Traffic Control | N/A | N/A | Executed 12/5/07 | \$200,000 | N/A |
| 1S3 | FA | Flagging & Traffic Control | N/A | N/A | Executed 7/2/08 | \$300,000 | \$300,000 |
| 13S1 | FA | PMIV Additional Funds (Resolved NOPC 7) | | | Executed 3/17/08 | \$300,000 | \$300,000 |
| 45 S1 | LS | Additional SWPPP | I&A 12/14/07 | N/A | Executed 1/31/08 | \$350,000 | N/A |
| 51 | LS | NOPC 12 & 13 Resolution | N/A | N/A | Executed 8/17/06 | \$25,234 | N/A |
| 52 | 0 | Elimination of Contractor's Design of Tie-Ins | I&A 1/19/07 | N/A | Executed 3/2/07 | \$0 | N/A |
| 53 | FA | Handling and Storage of Material | I&A 11/06/06 | N/A | Executed 12/8/06 | \$240,000 | N/A |
| 56 | LS | Contractor's Design additional cost... Resolved NOPCs 2,3,4,8,9,10,11,14, and 16 | | Approved 3/5/08 | Executed 3/17/08 | \$6,837,310 | (\$162,690) |
| 57 | LS | Demolition of Building 206 | N/A | N/A | Executed 10/18/06 | \$22,378 | N/A |
| 57S1 | LS | Remove and Clear Building 254 | N/A | N/A | Executed 6/4/07 | \$10,572 | N/A |
| 66S1 | FA | Video/Photo Documentation Services Supplemental Funds | N/A | N/A | Executed 4/14/08 | \$200,000 | \$200,000 |
| 86 | LS | Additional Suspension Costs | N/A | N/A | Executed 5/19/08 | \$42,764 | (\$57,236) |
| 91 | LS | Contract Days Extension/TRO Compensation to November 08 | RPP 8/28/07 | TBD | Executed 10/31/07 | \$1,818,948 | N/A |
| 91 S1 | LS | Base Contract TRO Extension to September 1, 2009 | I&A 10/25/07 | Approved 10/30/07 | Executed 11/16/07 | \$8,463,159 | \$0 |
| 91 S2 | LS | Global TRO adjustment and Base Contract TRO extension to December 31, 2009 | | TBD | In Progress | \$28,600,000 | \$0 |
| 96 | FA | SWPPP Steep Slope Stabilization Measures | N/A | N/A | Executed 1/4/08 | \$190,000 | \$0 |
| 96S1 | FA | Add Funds Shotcrete Slope at Bent 48 | N/A | N/A | Executed 7/2/08 | \$40,000 | \$40,000 |
| 109 | FA | MEP Coordination | N/A | N/A | Executed 1/30/08 | \$100,000 | \$0 |
| 110 | FA | Geotech. Exploration Pads and Support | N/A | N/A | Executed 2/20/08 | \$150,000 | \$50,000 |
| 119 | FA/LS/ID/UP | Project Wide SWPPP | I&A 4/07/08 | N/A | Executed 4/17/08 | \$638,939 | \$638,939 |
| 123 | FA | Treasure Island Yard Lot Rental | I&A 4/16/08 | N/A | Executed 4/17/08 | \$600,000 | \$600,000 |
| 125 | FA | Project Access Paving | | N/A | Executed 4/04/08 | \$150,000 | \$150,000 |
| 125S1 | FA | Additional Funds, Project Access Paving | I&A 6/12/08 | N/A | Executed 4/25/08 | \$35,000 | \$35,000 |
| 130 | LS | Project Retention | I&A 4/07/08 | N/A | Executed 4/14/08 | \$136,510 | \$136,510 |
| 131 | | Permanent Erosion Control | | N/A | In Progress | \$ 200,000 | \$200,000 |
| 132 | LS | Storm Damage Slope Repair (Resolved NOPC 17) | | N/A | Executed 5/23/08 | \$23,870 | \$23,870 |
| 142 | FA | Macalla Road Sinkhole Repair | | N/A | Executed 7/18/08 | \$150,000 | \$150,000 |
| 146 | FA | Macalla Road Tree Trimming | N/A | N/A | Executed 7/21/08 | \$50,000 | \$50,000 |
| | | Non CCO Charges...COZEPP, lead survey, respirator training | | | In Progress | \$1,323,000 | \$0 |
| Current Status for Administrative and General CCOs | | | | | | \$51,197,684 | \$2,654,393 |

Budget Status

As of January 2008 the revised additional cost estimate for Time Related Overhead, escalation issues, and job wide changes is \$48.6M with a current projection at \$51.2M. As Contract Change Orders for these items are negotiated, this estimate will be updated. Costs related to settlement of NOPC issues not captured here will be paid out of the contract contingency.

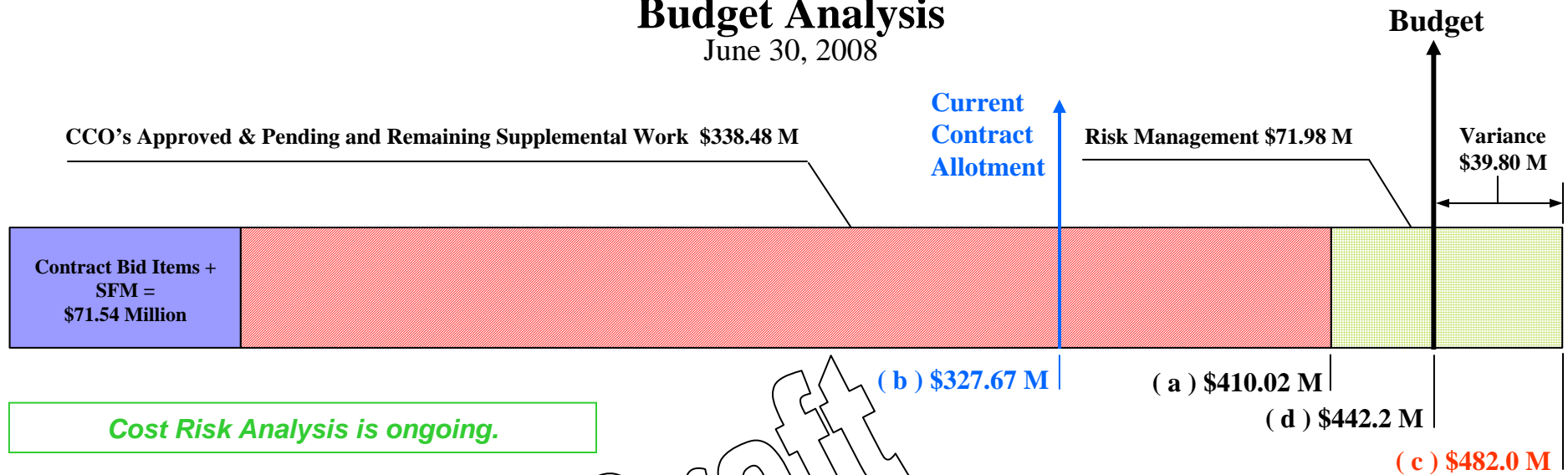
Additionally, the original contract allotment provided \$1.3M for COZEED. Subsequently, there were \$23,000 in other charges for a lead survey and respirator training both related to the WTI Phase 1 demolition work, providing for total non-CCO related charges of \$1.323M to the contract. These costs are shown here to capture costs to the project. It is also important to note that with two full bridge closures planned additional COZEED funds may be required.

CCOs executed to date are \$21M.

South-South Detour Contract 04-0120R4

Budget Analysis

June 30, 2008



Contract 04-0120R4 YBISouth-South Detour
Current Contract Budget Funding Status
 June 30, 2008 Basis

| | | |
|---------------------------------------------------------------------------------------|----|-------------------|
| Contract Bid Items | \$ | 71,159,650 |
| State Furnished Materials (SFM) | \$ | 379,000 |
| Subtotal | \$ | 71,538,650 |
| Supplemental Work | \$ | 14,115,000 |
| Contingency At 5% | \$ | 4,266,350 |
| Subtotal Original Contract Allotment | \$ | 89,920,000 |
| Supplemental Budget Allocation Approved | \$ | 237,747,000 |
| Subtotal Current Contract Allotment | \$ | 327,667,000 (b) |
| Remaining Unallotted Budget (Current Contract Budget - Current Contract Allotment) | \$ | 114,533,000 |
| Total Current Contract Budget | \$ | 442,200,000 (d) |

Reported Total Forecast At Completion \$ 461,200,000
 In 1st Quarter 2008 TBSRP Report

Contract 04-0120R4 YBISouth-South Detour
Contract Forecast At Completion (FAC) & Variance
 June 30, 2008 Basis

| | | |
|-----------------------------------------------------------|----|-------------------|
| Contract Bid Items | \$ | 71,159,650 |
| State Furnished Materials (SFM) | \$ | 379,000 |
| Subtotal | \$ | 71,538,650 |
| Supplemental Work Remaining | \$ | - |
| CCO's Baseline (Approved (48) + Pending (1) = Total 49) | \$ | 12,082,527 |
| CCO's (Approved (86) + Pending (31) = Total (117)) | \$ | 326,398,615 |
| CCO's = or > \$1 Million Pending POC approval | \$ | - |
| Subtotal | \$ | 410,019,792 (a) |
| Risk Management Costs | \$ | 71,976,000 |
| Total | \$ | 481,995,792 (c) |

Variance (Total - Current Budget) \$ 39,795,792

Confidential Draft – For Deliberative Purpose Only

ITEM 5: SAN FRANCISCO-OAKLAND BAY BRIDGE UPDATES

- c. Yerba Buena Island Transition Structures
(YBITS) No. 1
 - 1) Update (matrix)

Memorandum

TO: Toll Bridge Program Oversight Committee (TBPOC) **DATE:** August 27, 2008

FR: Tony Anziano, Toll Bridge Program Manager, Caltrans

RE: Agenda No. - 5c

Item- San Francisco-Oakland Bay Bridge Updates

Yerba Buena Island Transition Structures No. 1

Recommendation:

For Information Only

Cost:

N/A

Schedule Impacts:

N/A

Discussion:

The matrix on the following page is provided as a tracking tool for the specification elements contained in the Yerba Buena Island Transition Structures No. 1 contract.

Attachment(s): N/A

Memorandum

| Subject | Method for Incorporation into Project | | Notes |
|---------------------------------------------|---------------------------------------|------------------------|------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|
| | Bid Documents | Addendum / CCO / Other | |
| Roadway and Structure Plans | ✓ | | Roadway and structures plans are complete and advertised. |
| A + B Bidding | ✓ | | Incorporated into the contract as advertised. The B time includes completing work up to 12 meters before hinge K with a maximum of 900 days at \$50,000 per day. |
| Bid opening date | | ✓ | Bid opening date may require an extension. See detailed discussion in Opportunity Schedule Update. |
| Areas for Contractors use (Areas PR and FP) | ✓ | ✓ | To minimize contractor congestion on the island, the start of field work for YBITS #1 must be coordinated with completion of work by C.C. Myers. Current update to the Opportunity Schedule indicates that C.C. Myers may not clear the area until April 2010. Removed work restriction on the area around hinge K to allow for maximum amount of work to occur. Potential risk that ABF will need area to construct SAS. |
| Demolition of existing bridge | | ✓ | This work is currently in the C.C. Myers contract; however, it may be possible to place this work in YBITS 1 should that make the most sense from a scheduling and cost perspective. |
| W5 foundation and column | | ✓ | There is a provision to remove this work from the CCO with C.C. Myers. This work can be placed back in YBITS 1 should that make the most sense from a scheduling and cost perspective. |
| Falsework ownership | | ✓ | If the structures built during YBITS 1 cannot be stressed they may need to remain on falsework for an extended period of time, which would make Department ownership of the falsework desirable. |

Memorandum

| Subject | Method for Incorporation into Project | | Notes |
|---------------------------------|---------------------------------------|------------------------|--------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|
| | Bid Documents | Addendum / CCO / Other | |
| Alternative construction method | | ✓ | <p>Add a hinge to the YBITS 1 contract</p> <p><u>Pros:</u></p> <ol style="list-style-type: none"> 1. Avoids conflict in Area FP with ABF. 2. Allows for independent stressing of frames and decoupling this work from SAS contract. 3. May avoid need for more substantial falsework <p><u>Cons:</u></p> <ol style="list-style-type: none"> 1. Currently not designed in contract. 2. Complicated change that could significantly delay the project |

**ITEM 5: SAN FRANCISCO-OAKLAND BAY
BRIDGE UPDATES**

d. Oakland Touchdown (OTD) No. 1

1) Update

Memorandum

TO: Toll Bridge Program Oversight Committee (TBPOC) **DATE:** August 27, 2008

FR: Tony Anziano, Toll Bridge Program Manager, Caltrans

RE: Agenda No. - 5d1

Item- San Francisco-Oakland Bay Bridge Updates

Oakland Touchdown (OTD) No. 1 Update

Recommendation:

For Information Only

Cost:

N/A

Schedule Impacts:

N/A

Discussion:

A verbal update on the status of the Oakland Touchdown (OTD) No. 1 contract will be provided at the meeting.

**ITEM 5: SAN FRANCISCO-OAKLAND BAY
BRIDGE UPDATES**

e. Bridge Aesthetics Update

Memorandum

TO: Toll Bridge Program Oversight Committee (TBPOC) **DATE:** August 27, 2008

FR: Clive Endress, Senior Landscape Architect, Caltrans

RE: Agenda No. - 5e
Item- San Francisco-Oakland Bay Bridge Updates
Bridge Aesthetics Update

Recommendation:

For Information Only

Cost:

N/A

Schedule Impacts:

N/A

Discussion:

Bridge aesthetics stem back to the Engineering and Design Advisory Panel (EDAP), which was formed in 1997 to advise an MTC Task Force on issues of bridge design factors along with costs, engineering feasibility, and seismic safety. The aesthetic goal for the new East Span has been to provide design consistency from shore to shore. The design intent has been for a simple and sleek structure, punctuated by a signature main span, that would reveal itself as a bridge of the 21st century.

EDAP and the design team envisioned the bridge as a “white line” across the Bay. All vertical elements, including the tower, piers, and light standards, were designed with faceted forms to emphasize the clean modern lines of the structure and intensify the effects of light and shadow.

Currently, there are two issues concerning bridge aesthetics:

1. Color of concrete/steel
2. Light pipe

Color Contrast

Originally both the steel and concrete portions of the bridge were envisioned as white; however, additional funding for white concrete was not secured. Options for concrete coloring are still being explored.

As illustrated in Figures 1 and 2, recent painting of the OBG portion of the Skyway has highlighted the color contrast between the steel portions of the structure that will be painted white, and the concrete portions, which will remain light grey. Figure 2 illustrates the eventual final contrast between the white SAS and grey Skyway and YBITS.

Table 1 presents three options for consideration. Generally, these options include painting all, sections or the outside edges of the concrete in the Skyway, OTD, and YBITS white. Forthcoming capital and lifecycle costs will be important criteria in making a final decision.

Figure 1: White Painting of OGB



Figure 2: Simulation of SAS



Table 1: Three Options for Bridge Color

| Option | | Description |
|--------|-----------------------------------|----------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|
| 1 | Stay the Course | The bridge would look different from the north as compared to the south due to the bike path on the southern side being painted white. |
| 2 | Paint Limited Portions | Paint the winged portion of the concrete Skyway, OTD, and YBITS white, as well as the exterior side of concrete barriers for the entire structure, to ensure visual continuity throughout the structure. |
| 3 | Paint a Transition Portion | Paint a “transition” portion between the SAS and Skyway, which would lessen the contrast gradually. |

Light Pipe

Another aspect of the “white line” across the Bay relates to the proposed light pipe, which would span the outside edges of the East Span to unify the appearance of the bridge and add to its distinctiveness. The light pipe would provide aesthetic, nighttime lighting to complement the daytime “signature” of the new bridge. Figure 3 presents a simulation image of the light pipe.

Figure 3: Nighttime East Span with Light Pipe



BAMC conducted an Architectural Lighting Review in Fall 2007, evaluating the technical feasibility and constructability of the light pipe for YBITS, SAS, Skyway and OTD, and developing rough order-of-magnitude (ROM) costs. Updated cost estimates show a range from \$29M to \$41M to install the light pipe.

The review concluded that the overall constructability of the light pipe design is achievable with minimal change orders to existing or future contracts. Maintenance costs would include the replacement of lamps every six years. The leading technology was reported to be Light Emitting Diode (LED), which has experienced recent growth in market share and technology advancement, and a concomitant decline in cost.

To test the feasibility of the light pipe, BAMC has proposed that a demonstration segment of pipe be installed on the Skyway. The estimated cost of this demonstration is \$500,000 and would depend on the length of the test pipe.

Attachment(s):

N/A

ITEM 6: NEW BENICIA-MARTINEZ BRIDGE

a) Tour

Memorandum

TO: Toll Bridge Program Oversight Committee (TBPOC) **DATE:** August 27, 2008

FR: Mo Pazooki, Project Manager, Caltrans
Peter Lee, Senior Transportation Engineer, BATA

RE: Agenda No. - 6a

Item- Existing Benicia-Martinez Bridge Modification Contract Update

Recommendation:

For Information Only

Cost:

N/A.

Schedule:

N/A.

Discussion:

The Department and its contractor have completed the first phase of the project. Work completed include deck rehabilitation work along the east side of the bridge, demolition of the old toll plaza, repairs on the undulations just south of the Marina Vista Interchange, and the realignment of southbound Interstate 680 onto the bridge. Over the weekend of August 15, 2008, southbound I-680 and eastbound I-780 were shifted onto the east side of the bridge to allow for the remaining work on west side.

Future work includes replacing a 12 feet strip of deck along the entire west side of the bridge, on-going deck rehabilitation as determined after closer inspection, lifting of the west half of the Marina Vista Blvd overcrossing to match final roadway grades, continuing undulation repairs, and adding of a separation barrier across the entire span for the pedestrian/bicycle pathway. The work is anticipated to be completed in the fall of 2009.

The TPBOC will be taken on a tour of the project by Department construction staff at the end of the meeting on September 4, 2008.

ITEM 7: OTHER BUSINESS

No Attachments